



AGENDA
CHARTER TOWNSHIP OF MERIDIAN
PLANNING COMMISSION – REGULAR MEETING
August 26, 2019 7PM

1. CALL MEETING TO ORDER
2. PUBLIC REMARKS
3. APPROVAL OF AGENDA
4. APPROVAL OF MINUTES
 - A. August 12, 2019 Regular Meeting
5. COMMUNICATIONS - listed on separate page
6. PUBLIC HEARINGS - None
7. UNFINISHED BUSINESS
 - A. Special Use Permit #19111 (Woodward Limited Dividend Housing Association), develop 49 unit apartment complex with four buildings on north side of Sirhal Drive, west of Greencliff Drive.
 - B. Special Use Permit #19121 (Woodward Limited Dividend Housing Association), construct group of buildings greater than 25,000 square feet in size on north side of Sirhal Drive, west of Greencliff Drive.
 - C. Special Use Permit #19101 (LaFontaine Chrysler, Dodge, Jeep, & Ram of Okemos), construct 24,902 square foot new car dealership at 1510 Grand River Avenue.
 - D. Wetland Use Permit #19-03 (LaFontaine Chrysler, Dodge, Jeep, & Ram of Okemos), discharge storm water to regulated wetland at 1510 Grand River Avenue.
 - E. Special Use Permit #19-74011 (Michigan Montessori), appeal of approved special use permit to add 0.50 acre parcel to Montessori Radmoor School property at 2745 Mt. Hope Road.
 - F. Preliminary Plat #19012 (Giguere Homes), proposed seven lot subdivision titled Sanctuary 3 located on the north side of Robins Way, east of Hulett Road.
8. OTHER BUSINESS
 - A. Form based code initiative.
9. REPORTS AND ANNOUNCEMENTS
 - A. Township Board update.
10. PROJECT UPDATES
 - A. New Applications - None
 - B. Site Plans Received - None
 - C. Site Plans Approved - None
11. PUBLIC REMARKS
12. ADJOURNMENT
13. POST SCRIPT: HOLLY CORDILL

AGENDA page 2
CHARTER TOWNSHIP OF MERIDIAN
PLANNING COMMISSION MEETING
August 26, 2019 7:00 pm

TENTATIVE PLANNING COMMISSION AGENDA
September 9, 2019

1. PUBLIC HEARINGS
 - A. None

2. UNFINISHED BUSINESS
 - A. None

3. OTHER BUSINESS
 - A. Form based code initiative.

Individuals with disabilities requiring auxiliary aids or services should contact: Principal Planner Peter Menser, 5151 Marsh Road, Okemos, MI 48864 or 517.853.4576 - Ten Day Notice is Required.
Meeting Location: 5151 Marsh Road, Okemos, MI 48864 Township Hall

Providing a safe and welcoming, sustainable, prime community.



**CHARTER TOWNSHIP OF MERIDIAN
PLANNING COMMISSION
REGULAR MEETING MINUTES**

DRAFT

**August 12, 2019
5151 Marsh Road, Okemos, MI 48864-1198
517-853-4560, Town Hall Room, 7:00 P.M.**

**PRESENT: Commissioners Lane, Scott-Craig, Hendrickson, Trezise, Premoe and Richards
ABSENT: Commissioners Shrewsbury and Cordill
STAFF: Director of Community Planning & Development Mark Kieselbach and
Principal Planner Peter Menser**

1. Call meeting to order

Chair Scott-Craig called the regular meeting to order at 7:01 P.M.

2. Public Remarks

- A. Jessi Adler, 1580 Hillside Drive, spoke in opposition to Special Use Permit #19101 and Wetland Use Permit #19-03.
- B. Jim Gigure, 6253 Fenwick Court, introduced himself and said he will be available for questions during the Preliminary Plat #19012 Public Hearing later in the meeting.
- C. Lynne Page, 3912 Raleigh Drive, spoke in opposition to Rezoning #19060.

3. Approval of Agenda

Commissioner Richards moved to approve the agenda as written.
Seconded by Commissioner Trezise.
VOICE VOTE: Motion approved unanimously.

4. Approval of Minutes

- A. July 22, 2019 Regular Meeting

Commissioner Trezise moved to approve the minutes as written.
Seconded by Commissioner Hendrickson.
VOICE VOTE: Motion approved unanimously.

5. Communications

Chair Scott-Craig noted the communications listed in the meeting packet and said hard copies of the communications received after the meeting packet was sent out were assembled and distributed to the Planning Commission at their places on the dais prior to the meeting.

6. Public Hearings

- A. Special Use Permit #19101 (LaFontaine Chrysler, Dodge, Jeep, & Ram of Okemos), construct 24,902 square foot new car dealership at 1510 Grand River Avenue.

Chair Scott-Craig opened the public hearing at 7:12 P.M.

Principal Planner Menser provided an overview of the proposal and noted he would be available to answer questions.

Applicant Gary Laundroche, 4000 W. Highland Road provided additional information about the proposed project and discussed the advantages of their business coming to the area verses other types of businesses who could come into the area under C2 zoning and talked about the advantages of LaFontaine's hours of operation, noise reduction procedures and lighting considerations made to accommodate surrounding residents. In addition, LaFontaine is supportive of the community in such gestures as funding for the Future Farmers Market and will be employing approximately 75 employees. Mr. Laundroche noted Wayne Dubois was also in attendance from LaFontaine and they were both said they were available to answer questions.

Mr. Laundroche noted there are 144 trees existing on the property currently and after the land cut and fill grading is completed 82 trees will remain. He also stated nothing is being developed in the wetland area.

Public Comments:

- A. Jessi Adler, 1580 Hillside Drive, spoke in opposition to Special Use Permit #19101 and Wetland Use Permit #19-03.
- B. Ari Adler, 1580 Hillside Drive, spoke in opposition to Special Use Permit #19101 and Wetland Use Permit #19-03.
- C. Cecelia Kramer, 4560 Oakwood, spoke with concern regarding Special Use Permit #19101 and Wetland Use Permit #19-03.
- D. Barbara Loyer, 1445 E. Pond Drive, spoke in opposition to Special Use Permit #19101 and Wetland Use Permit #19-03.

Planning Commission Discussion:

- The staff reviews photometric light plans. The lighting plan language will be added as a condition as approved in the other LaFontaine plan.
- The applicant agreed to submit a letter before the next Planning Commission meeting responding to the 9 criteria/standards listed in the Special Use Permit application.
- Test driving in the neighborhood has been an issue brought up by numerous residents and the applicant stated the employees would be trained by management through an internal policy at LaFontaine to inform the test drivers not to drive in the local neighborhood.
- There will be an internal connection road constructed between LaFontaine and the Sparrow Health System building to the west.
- The 50 foot tapered turn lane is in the design criteria and is currently being reviewed by the Michigan Department of Transportation.
- The green and screen appearance from Grand River will have pods of dense plantings to break up the car lot.

- The site plan review is very detailed and LaFontaine would be willing to consider future Form Base Code updates.

A straw poll indicated the Planning Commission would be in support of approving Special Use Permit #19101 with conditions at the next meeting.

Chair Scott-Craig closed the public hearing at 8:14 p.m.

- B. Wetland Use Permit #19-03 (LaFontaine Chrysler, Dodge, Jeep, & Ram of Okemos), discharge storm water to regulated wetland at 1510 Grand River Avenue.

Chair Scott-Craig opened the public hearing at 8:15 P.M.

Principal Planner Menser provided an overview of the proposal and noted he would be available to answer questions.

The applicant, Gary Laundroche, 4000 W. Highland Road, with LaFontaine discussed the proposal and said he was available to answer questions.

Public Comment:

- A. Jessi Adler, 1580 Hillside Drive, expressed concerns with paving Powell Road, how the water flow will be directed following construction and what the impact might be on their well for water.

A straw poll indicated the Planning Commission would be in favor of supporting Wetland Use Permit #19-03 at a future meeting.

Chair Scott-Craig closed the public hearing at 8:37 p.m.

- C. Special Use Permit #19-74011 (Michigan Montessori), appeal of approved special use permit to add 0.50 acre parcel to Montessori Radmoor School property at 2745 Mt. Hope Road.

Chair Scott-Craig opened the public hearing at 8:38 P.M.

Director of Community Planning & Development Mark Kieselbach provided an overview of the appeal and said he was available to answer questions.

The appellant, Sergey Baryshev, 2767 Mount Hope Road, provide a presentation to appeal the approval of the minor amendment to Montessori Radmoor School's Special Use Permit #19-74011.

The applicant, Irina Jamison, provided background on the school's history and said she was available to answer questions.

Public Comment: None

Planning Commission Discussion:

- Focus on the approval of the minor amendment and not a reconsideration of the original Special Use Permit and if it is complying with its terms and conditions brought up by the appellant during his presentation to the Planning Commission.
- The Special Use Permit goes with the land and the use is open space as proposed at this time is to remain green grass area.
- Adding property is a minor amendment under the ordinance and anything else would be another amendment.

A straw poll indicated the Planning Commission would be in favor of affirming the decision of the Director of Community Planning and Development without modifications its next meeting.

Chair Scott-Craig closed the public hearing at 9:28 p.m.

The Planning Commission took a 5 minute recess.

- D. Preliminary Plat #19012 (Giguere Homes), proposed seven lot subdivision titled Sanctuary 3 located on the north side of Robins Way, east of Hulett Road.

Principal Planner Menser provided a review of the proposal.

The applicant, Jim Giguere, 6253 Fenwick Court, said he was available to answer questions.

The Planning Commission agreed to consider a resolution to recommend approval of the plat at its next meeting.

Public Comment: None

Chair Scott-Craig closed the public hearing at 9:39 p.m.

7. Unfinished Business

- A. Special Use Permit #19111 (Woodward Limited Dividend Housing Association), develop 49 unit apartment complex with four buildings on north side of Sirhal Drive, west of Greencliff Drive.
- B. Special Use Permit #19121 (Woodward Limited Dividend Housing Association), construct group of buildings greater than 25,000 square feet in size on north side of Sirhal Drive, west of Greencliff Drive.

Principal Planner Menser stated both items would run concurrently and provided an update. He also noted a revised site plan was received late in the week so additional comments will be available at the next Planning Commission meeting on August 26, 2019.

Applicant Frank Fugate, 500 South Front Street Columbus, Ohio, with Woda Cooper Company provided additional information on the updated site plan prepared by Monument Engineering Group Associates and said he was available to answer questions.

Planning Commission Discussion:

- The Planning Commissioners noted appreciation with the updated engineering plan design and thanked the applicant for all of the work involved.
- The list of 10 variances has been narrowed down to 2 variances since the developer met on July 22, 2019.

A straw poll indicated the Planning Commission would recommend approval for Special Use Permit #19111 and Special Use Permit #19121 at its next meeting.

8. Other Business - None

9. Reports and Announcements

A. Township Board updates.

Chair Scott-Craig noted there was no Economic Development Corporation meeting scheduled for August 2019.

Principal Planner Menser provided an update from the Township Board's recent agenda.

10. Project Updates

- A. New Applications - None
- B. Site Plans Received - None
- C. Site Plans Approved
 - 1. Site Plan Review #19-08 (Haslett Road Holding LLC), construct 88 lot Copper Creek development on north side of Haslett Road, east of Creekwood Lane.

11. Public Remarks

- A. Irina Jamison, 2745 Mount Hope Road, with Michigan Montessori International School made additional comments regarding Special Use Permit #19-74011.
- B. Appellant Sergey Baryshev, 2767 Mount Hope Road, expressed concern with Special Use Permit #19-74011.

12. Adjournment

Chair Scott-Craig moved to adjourn the meeting.

Supported by all.

VOICE VOTE: Motion carried unanimously.

Chair Scott-Craig adjourned the regular meeting at 10:07 P.M.

Respectfully Submitted,

Debbie Budzynski, Recording Secretary

Communications Received
Meridian Township Planning Commission
Monday, August 26, 2019

1. Fred Hawley RE: Special Use Permit #19101
2. Leslie Johnson RE: Special Use Permit #19101
3. Amy Blair RE: Special Use Permit #19101
4. Ralph Pyle RE: Special Use Permit #19101
5. Wei Li RE: Rezoning #19060
6. Laura Carter RE: Special Use Permit #19101
7. Carla Galligan RE: Rezoning #19060
8. Charles Kotz RE: development
9. Yingxin Zhou RE: medical marihuana
10. Carolyn Sebestyen RE: Rezoning #19060
11. Julia White RE: Special Use Permit #19101
12. Serget Barshev & Evgenia Barysheva RE: Special Use Permit #19-74011
13. Champion Woods Homeowners Association RE: Rezoning #19060
14. Yingxin Zhou RE: Rezoning #19060
15. Yingxin Zhou RE: Rezoning #19060

Peter Menser

From: Frederick Hawley <hawley.fj@sbcglobal.net>
Sent: Friday, August 09, 2019 3:45 PM
To: Planning Commision (DG)
Subject: SUP 19101 LaFontaine

To the Planning Commission, Charter Township of Meridian:

You have received many communications, and will hear comments during the public hearing, opposing the special use permit 19101 requested by LaFontaine Automotive Group. I am in agreement with this opposition and unable to be present for the public hearing. I hold no hope, however, for denial of this application.

The Planning Commission is the last hope for residents of the township, especially those living and owning residential property near the proposed dealership, for protections of property values and living conditions that were assumed to be better than the effects this development may bring about.

The very minimal conditions which must be put in place with an approval of this permit are:

1) Lighting must be directed downward and shielded to contain the lighting within the applicants property, requiring lower levels of lighting from a height of 8 feet or less during hours not open for business, and with motion detectors if security lighting is deemed necessary. This is necessary to provide relief from excessive lighting to residents on the south side of Grand River Ave.

2) Outdoor sounds from public address or music must be prohibited.

3) Provisions must be made to protect adjacent residential and commercial properties from storm water runoff and resulting damages. The Walden Pond residents currently have water problems which are likely to increase by paving areas shown in the applicants plans. This project also should not interfere with the needed improvements to the Daniels Drain.

I would also hope you will see the benefits of maintaining maximum green space, preserving mature trees, and minimized areas of impervious paving in your consideration of this application. Restrictions on test drives in residential areas also seem reasonable.

I remind you that only at this time can these conditions be put in place to protect nearby property owners now and in the future. Promises sincerely made by the applicant at this time to comply with requests and desires of surrounding homeowners have no value in the future if there are no conditions contained in the approval by the commission. Future changes in business models or changes in the business ownership should not allow activities on this property to adversely affect owners of residential properties. We need these conditions as a basis for enforcement, if necessary, in the future.

Thank you for your attention to these requests.

Fred Hawley
4543 Eastwood Dr.

August 09, 2019

Re: Special Use Permit #19101

Dear Planning Commissioners and Planning Department,

I strongly object and urge you to reject/vote no on the special use permit and wetland use permit for properties 1510/1560 Grand River Ave, a planned LaFontaine Automotive Group dealership.

I have resided in the Meridian Township for over 20 years, and have been very disturbed by the increasing development taking place in the Haslett/Okemos area in recent years. Development will continue, however, when I chose to purchase a home in Haslett, I believed that "protected land areas" meant just that. Unfortunately, in recent years, I have witnessed the encroachment on wetlands, and other areas of the wetland being destroyed. The once natural drive on Cornell has almost disappeared, only to be replaced by housing, bulldozers, noise pollution and heavy traffic.

I am not a rabid environmentalist. I do care about quality of life though, and I would like to trust that our township is seriously considering its residents before allowing a major auto dealership to replace some of the remaining vestiges of nature.

Thank you,

Leslie Johnson
1144 Cliffdale Drive
Haslett, MI 48840

Peter Menser

From: Amy Blair <blaira@msu.edu>
Sent: Friday, August 09, 2019 11:50 AM
To: Planning Commission (DG)
Cc: Amy Blair
Subject: Special Use Permit #19101

August 2019
Re: Special Use Permit #19101

Dear Planning Commissioners and Planning Department,

I strongly object and urge you to reject/vote no on the special use permit and wetland use permit for properties 1510/1560 Grand River Ave, a planned LaFontaine Automotive Group dealership.

Is this really the look you want for the entrance to the township? A long row of cars? That doesn't scream "Welcome to Meridian Township" to me. At least not the welcoming, progressive, homey, environmentally friendly, walkable community you say it is.

Along with our neighbors, we have watched this township continue to rezone acres upon acres of rural residential property into commercial zones and allow way too many commercial entities to move in. Many of us purposely bought houses in the more rural sections of the community. Now, we are being subjected to decreased property values as well as increased traffic, light pollution, noise, deforestation, and displacement of wildlife. The words "they paved paradise and put up a parking lot" couldn't be more true for what this planning commission is doing to our township, specifically if you allow yet another car dealership. We do not need more pavement, congestion, noise, pollution and ugliness.

A car dealership will bring all of those items I just mentioned and negatively impact our daily lives and house values. Listed below are my major concerns and reasons why you should reject this special use permit.

- 1) **Light Pollution.** Car dealerships are known to have more light poles than any business ever needs. Our relatively dark sky area will now be lit up like a major city.
 - a. Light pollution is an increasing problem globally and we need to stop this. Why does a dealership need to be lit up like a runway 24-hours a day? If they're concerned with security there are other ways to handle this, without having all the lights on. Even the new LEDs and downward facing or angled lights aren't enough – people are still shorter than a light pole so we would be subjected to seeing these lights while driving down the road or looking to the night sky.
 - b. Research has shown that bright lights, like the ones often used at car dealerships, contribute to negative changes in circadian rhythms, disrupting natural cycles for both humans and animals. Being subjected to these types of lights late at night would be detrimental to those of us who live nearby and the wildlife who inhabit the nearby woods.
- 2) **Wetland destruction.** This parcel includes wetlands, which are protected under state and federal law. In fact, Michigan's wetlands law recognizes the important benefits provided by wetlands and their vital role in recreation, flood control, wildlife habitat, recharging groundwater supplies, erosion

control, and more. Yet, Meridian Township seems intent on destroying wetlands in favor of pavement, which does not contribute to any of those vital things.

3) **Deforestation.** Grass and trees are necessary to cool the earth, produce much-needed oxygen, and help with water runoff and drainage. The wholesale removal of large mature trees that they will attempt to replace with tiny sticks that do not even cast a shadow is alarming. We must act now to save these mature trees. The site plan shows teeny pockets of grass with little to no trees or shrubs dispersed on islands throughout the parking lot. That is not enough to counteract the vast amount of pavement planned. Again, for a township that has a [Greenspace Plan](#), you sure are doing a good job of trying to get rid of all of it.

A [recent study](#) shows tree cover in the US is shrinking and includes these quotes:

- *"If we continue on this path, cities will become warmer, more polluted and generally more unhealthy for inhabitants,"* said David Nowak, a senior US Forest Service scientist and co-author of the study.
- *But the one reason for tree loss that humans can control is sensible development. "Every time we put a road down, we put a building and we cut a tree or add a tree, it not only affects that site, it affects the region,"* Nowak says.

a. New building options include grass parking lots that are permeable and allow for natural features to be maintained while protecting the environment. JoAnn Forsberg is a local developer who has considered using this type of green parking and I think it's something to consider.

b. Have you considered working with LaFontaine and other parties in town to allow them to build on sections of land already paved over, such as the Meridian Mall or Toys R Us? That parking lot sits empty now most of the time and would be an ideal location for a car dealership that desires lots of pavement. This would prevent additional deforestation and greenspace destruction.

c. Most people know what car they want before they step foot at a dealership, and many buy cars online. Why on earth does LaFontaine need such a huge paved space to park hundreds of cars? The wave of car buying is changing, yet this township is living in the past. If you want to build a new community, the time is now to start making the necessary changes to allow for non-traditional development and site planning.

4) **Wildlife destruction.** A study released recently by the [World Wildlife Foundation](#) says that in the past 40 years, humans have been responsible for causing the wildlife populations to fall by more than half due to deforestation and pollution – both things a car dealership will contribute to. We already have a large deer herd in this area, and it is well-known they live in the woods on Powell and cross regularly into Forest Hills and Walden Ponds and The Shoals. Where are those deer going to go if you eliminate their entire forest? And not just the deer, but the rest of the wildlife that live there and who have made that their home much longer than any of us? Deforestation up and down Powell and Grand River has caused more and more animals to move into our neighborhoods because they have nowhere else to go. As the study says *"We're facing a rapidly closing window for action and the urgent need for*

everyone—everyone—to collectively rethink and redefine how we value, protect, and restore nature.” The township is already paying hunters to unnecessarily reduce the deer population in the township that is seen as a threat. But the deer wouldn't be a threat if we would stop removing the natural areas they want to live in for the sake of more tax revenue.

5) **Increased traffic.** Grand River is heavily traveled and in the 24+ years we have lived in Meridian Township, we find it harder and harder every day to exit our subdivision due to increased traffic from the urban sprawl east of us. Almost exactly at the corner of Northview and Grand River the speed limit changes to 50 mph. That will be right at the entrance to the LaFontaine dealership.

a. I'm concerned about the ability to get in and out of the subdivisions and crashes from cars trying to turn every which way. The curb cuts are already too close together and every day it's a serious crash waiting to happen when someone is turning out of Northview headed west and someone is trying to turn north into Sparrow, and someone else is trying to get in or out of Baryames. I have witnessed and been party to many near-misses. Adding a car dealership will add to an already bad situation.

b. We also are concerned about increased traffic in our neighborhoods from customers test-driving vehicles through it. Too many cars speed through our neighborhood already. We should not be subjected to additional traffic from people who aren't familiar with the area or the vehicle they are driving testing out cars and not paying attention to their surroundings. The past decade has seen an increase in families living in our subdivision and we are noticing more kids on foot and on bikes traveling through. They should not have their neighborhood streets turned into a test track.

c. Powell is designated as a scenic road corridor and yet continues to be pilfered for development. Residents who run or walk there have a hard time crossing Grand River to get there now due to it not being the most pedestrian-friendly area. I am concerned that a dealership on the corner with increased traffic and de-acceleration lanes will prevent people from being able to safely reach Powell on foot. This township continues to say it wants to be trailway and pedestrian friendly, but this proposed development is quite the opposite of that and will deter runners and walkers from heading down Powell.

6) **Water redirection.** Car dealers also need as much pavement as possible to park their cars on – pavement is not environmentally friendly because it generates heat, is not permeable, and contributes to global warming. Water that drains into the ground helps replenish the aquifer. If the ground is paved over, that water will be redirected and quite likely contaminated with runoff from the parking lot. As a household that is still on a well, this is extremely concerning to me as we rely on that water.

7) **Poor drainage and flooding.** You are aware that the Daniels Drain runs behind Forest Hills, under Walden Ponds, and north under Grand River. LaFontaine would contribute a lot of water to an already overburdened and failing drain system. This would be bad for everyone. Many of the condo units in Walden Ponds are already unusable because they flood regularly from the drain backing up.

8) **Decreased property values.** Realtors always tell you Location, Location, Location. Where a house is located is extremely important – and across the street from a large car dealership probably isn't the first location a family would choose when looking for a house or condo. It's not a business you frequent regularly, unlike a grocery store, pharmacy or restaurant. We were sold on our house because of its proximity to a grocery store, as well as how rural it was – there was nothing commercial to the east of us except the driving range, which is still rural because it's 99% green space. The land north and east was all zoned rural residential, which appealed greatly to us. Now it's been rezoned commercial and you want to add a car dealership to the mix – a business that isn't great for what was

supposed to be a semi-rural family neighborhood. Our property value has finally recovered after the recession and I worry significantly that it will drop again if this proposal is approved.

While I object to this proposal and strongly urge you to reject it based on the concerns listed above, I assume it is likely a foregone conclusion that you will be approving it.

Therefore, the following items need to be required as a condition of the special use permit:

- 1) Require LaFontaine to shut off all their parking lot lights at night, especially since they close at 6 p.m. most evenings. For security reasons, the lights could be on a motion sensor so they only go on if tripped. For lights that are on during evening business hours, require the lights to be covered, dimmed, and angled so light does not leave their property.

- 2) Require any maintenance facilities to be placed as far back from the road as possible to minimize sound. This includes any service bays, car wash, loading docks, etc. In addition, there should be requirements for bioswales or other measures to help filter the water runoff from this industrial space prior to it reaching the aquifer.

- 3) Do not allow any outside sound like a public address system, music or honking.

- 4) Require modern construction techniques such as “green pavement” instead of traditional asphalt.

- 5) Require preservation of the existing mature trees and wetlands by designing and constructing their parking lot and buildings around the established trees instead of cutting them down and planting new tiny ones. Design the site layout to cluster cars together, allowing for as much green space as possible to be saved and incorporated.

- 6) Require that cars not be lined up in a straight row along the front of the property. Planning Commissioner Scott-Craig said it best last time: “We don’t want people driving down the road looking at your cars lined up along the front row, we want their eyes on the road.” Put all the cars out back and the building at the front.

- 7) Require that test drives not be allowed in our neighborhoods

Again, we feel this project will be extremely detrimental to the environment, our property values, and quality of life. **We urge you to vote no** on this special use permit. Meridian Township was already special and continued approval of these types of permits is ruining that.

Sincerely,
Amy and Tom Blair
4200 Shoals Dr.

Peter Menser

From: Pyle, Ralph <pyler@msu.edu>
Sent: Friday, August 09, 2019 11:05 AM
To: Planning Commision (DG)
Subject: LaFontaine dealership

As a resident of the Forest Hills subdivision, I request that you vote No on the special use permit for the proposed LaFontaine auto dealership. We need to protect green space and guard against over development in our community. I think we have enough traffic congestion in the area. I am also concerned about light pollution that is associated with car dealerships.

Ralph Pyle
1664 Forest Hills Dr
Okemos, MI

Peter Menser

From: Wei Li <weidli2012@gmail.com>
Sent: Friday, August 09, 2019 10:55 AM
To: Board; Planning Commision (DG)
Subject: Rezoning #19060 Mayberry Homes

Dear Board and Planning commission,

I'm upset by how rezoning #19060 is being handled by the applicant and would like you to consider the following concerns.

At the Aug 8, 2019 board meeting when addressing a question from Treasurer Deschaine as to why the applicant submitted a blank section II of the application form for the July 8, 2019 planning commission meeting, Bob Schroeder, the Mayberry owner said it's due to **staffing issue** (11:18 of the discussion). At 19:00 min, Mark Kieselbach, the Community planning & development director, said that the applicant **wasn't given any time** to update the application to provide answers to questions on the rezoning criteria.

I want to point out that the planning commission asked the applicant to address the criteria on rezoning to RAA at the June 24, 2019 meeting and the applicant had **FOUR WEEKS** by July 22, the day when the planning commission denied the application, to satisfy the request from the commission. Why did the applicant submit a new application the day after the planning commission voted to deny the request? How could the applicant solve the staffing issue overnight? It appears to me the applicant tried to bypass the planning commission and public review. This should not be allowed and please have it sent back to the planning commission for full review.

Also there is an error on the application form submitted on 7/23/2019. It says the applicants are requesting to rezone 7 parcels of 96.74 acres in total from RAAA and RR to RAA. But our understanding is that partial of the 96.74 acres is already zoned RAA. Could you please have the application sent back to the applicant for corrections?

Respectfully yours,

Wei Li

Peter Menser

From: Laura Carter <swim80100@gmail.com>
Sent: Tuesday, August 13, 2019 11:42 AM
To: Planning Commision (DG)
Subject: LaFontaine

Hello,

I was at the meeting last night to listen to the discussion about the proposed LaFontaine car dealership on Grand River Avenue right in the middle of (undesignated) green space - the last green space before the onslaught of uninterrupted concrete for the next couple of miles along Grand River. Unfortunately, I'm like a deer in the headlights when it comes to public speaking, but I lay awake for a long time last night thinking about what I would have said. May I share? I promise I'll keep your reading time to three minutes.

So, we should be grateful that it's going to be a car dealership, rather than a "bar or strip club"? Really? I seriously doubt you would have approved anything like that. Oh yes, I feel much better about the destruction of the last bit of green space now that I know there won't be a bar or strip club there. A completely absurd justification, right off the bat.

It was totally unnecessary for LaFontaine Lackey #2 to show us ten views of the property. We know what and where the property is. That's why we were there.

LaFontaine donated to the Farmer's Market? Please tell me that strikes you as ironic to the point of absurdity.

The retaining pond? Do you honestly believe that the runoff from a car dealership and repair garage will be healthy for the wetlands behind the area?

The LaFontaine Lackeys seemed so excited about their "dark sky" lighting, and they really seemed eager to "work with" you to make sure it's the most awesome car dealership ever. As one of the speakers said, get it in writing.

At the end of the day, when you strip away all the bullshit, it comes down to you and your consciences. Destruction or life? It's one or the other. I wonder if any of you have children or grandchildren, and I imagine what you would tell them (but hopefully will feel too ashamed) when you make this official:

Child: "Daddy/Grandpa, where were you last night?"

You: "My friends and I were deciding to let a very rich man chop down the trees in a beautiful old woods and destroy the homes of all the animals that live there so that he can build a car store."

Child: "I see lots of car stores when I go to Meijer with Mama. And what will the animals who live there do? I learned in science class that trees breathe the bad air that cars make. Shouldn't we keep the trees?"

You: "Oh, the animals will go somewhere else." (You will not tell your child that there will be nowhere left for them to go, thus, they will die desperate, slow deaths. Or get hit by cars as they flee out into Grand River).

Child: "But what about the trees making the air clean? Don't you want clean air?"

You (to yourself): "Meh, I'll be dead before too long, so it won't be my problem. And my kids and grandkids...well...they can just deal with it."

Child: "Daddy?"

You: "No, I would rather have a car store than clean air. Sorry, babe."

Child: "???"

So your legacy will be to give away the last parcel of green space along Grand River Avenue to a car dealership. To champion more pollution and carbons, and to destroy the trees that keep them in check. If you pass this, you should be so ashamed of yourselves. You should not be able to look your child in the eyes and tell her that you love her.

And all so that some no doubt obscenely rich jerk can get even richer, and so that people who want their brand of cars won't have to drive all the way to south Lansing. Or to the OTHER SIDE OF POWELL ROAD! Heaven forbid.

On a side note, I heard this morning that Horrock's wanted to buy the abandoned L&L in Haslett, but that the planning board rejected it because it would present competition to other area grocery stores. Isn't that exactly what having yet another car dealership in a two-mile stretch would do? I thought competition was the backbone of capitalism. So we who live in Haslett have to see this huge, abandoned building and the huge, decaying parking lot every time we leave home. Thanks. And in the meantime, more death and destruction will be wrought for the sake of rich assholes getting even richer. I don't know what else to say but shame. Shame on you. Please. Have a conscience. Give a damn about what you're doing to the planet. To the future of your children and grandchildren. Stand up and do the right thing. The LaFontaines will be just fine.

Yours truly,
Laura Carter

Peter Menser

From: Carla Galligan <carlagalligan@gmail.com> on behalf of Carla Galligan <carlagall@comcast.net>
Sent: Tuesday, August 13, 2019 9:47 AM
To: syka@meridian.mi.us
Cc: Planning Commision (DG); Phil Deschaine; Patricia Herring Jackson
Subject: Bennett Road Holding LLC #19060

Dear Mr. Styka,

I have been involved with the rezoning request #19060 since October 2018. I have attended all the meeting, distributed flyers, written letters and been a part of several meetings on this matter. For that reason I would say I have a solid understanding of the issues and process Meridian Township has undertaken to review this rezoning application.

I along with several of my neighbors are very disappointed with the remark make by Mr. Kieselbach at the August 8, 2019 Board meeting. Mr. Kieselbach stated " the planning commission didn't give the applicant any time to update the application. Please see minute 19:00 of item 13A on the recording. <http://meridianmi.swagit.com/play/08082019-811> . The remarks Mr. Kieselbach made mislead the board and was a disservice to the Planning Commission. Mayberry's original application was received by Meridian Township on September 23, 2018. There were 11 months of opportunity for Mayberry to submit updates. The Planning Commission did the best they could with an INCOMPLETE application. Mayberry made a few revisions, bypassed the Planning Commission and sent another INCOMPLETE application to the Board for the August 8, 2019 meeting. .

I was very impressed with the level of detail and process undertaken by the Planning Commission in reviewing these incomplete applications. Due diligence was evident in the comments and concerns the Planning Commissioners made at the meeting. If I submitted a job application and it was not complete I would not be considered for a position. If a student does not answer a question on a test they get a 0 for the question. When incomplete applications for rezoning are sent to the Meridian Township Board they move forward with a low standard in favor of the developer.

Mr. Schroeder was not prepared and he knew he didn't have to be, so he ignored the meetings and submission of a complete application. This is obvious.

The Planning Commission deserves a clarification by Mr. Kieselbach. His remarks are simply not true. He shows a bias to the developer and is seen as rescuing the developer. This is not only my opinion.

I look forward to your response and Mr. Kieselbach's clarification.

Carla A. Galligan

Peter Menser

From: Meridian Township, MI <meridian-mi@enotify.visioninternet.com>
Sent: Friday, August 16, 2019 9:07 AM
To: Planning Commision (DG)
Subject: Overdevelopment in Township

Message submitted from the <Meridian Township, MI> website.

Site Visitor Name: Charles Kotz
Site Visitor Email: cdkotz73@gmail.com

Dear Planning Commission:

I can't help but notice that you have allowed the pendulum to swing towards more congestion and overdevelopment in the Township. Why is Mayberry Homes proceeding to "slash and burn" along one of our last rural roads, (Powell), when we have a slew of empty homes for sale standing empty? It's time for you to "put the brakes on"! I thought Powell Rd. was to be "the line in the sand" and the goal was to retain some semblance of rural character to the East of that line. Well, that's history now, with this hideous development underway. I would like you to stop rubber stamping the approval of these dismal cheap homes being built in the a Township, and instead focus on acquiring more land that can be saved.

You allowed the Panera Bread to clear cut a stand of mature oak trees on Grand River, and now they are abandoning that site. Just another empty retail building on Grand River.....Do your job!

Sincerely,

Charles D. Kotz

Peter Menser

From: Yingxin Zhou <zhou0824@gmail.com>
Sent: Monday, August 19, 2019 11:36 AM
To: Board; Planning Commission (DG); Peter Menser; Frank Walsh
Subject: Medical marijuana ordinances revisit

Hello board and planning commission,

It has been almost 3 months since the medical marijuana ordinances were adopted by the township board on 5/21/19.

Per board members' discussion at the 5/21/19 board meeting, in 3 months if we don't have any applicants for growers, processors, distributors, safety compliance centers and other categories that are allowed by the ordinances, Treasurer Deschaine will induce the motion to change the ordinances and change it back to zero or whatever the number is at that point.

As of the 8/9/2019 deadline to apply for the permits, 21 applications had been submitted. None of them are for growers or processors. There is only 1 application for safety compliance center, 1 application for secure transporter and the remaining are all for provisioning centers. So could the board build a consensus to eliminate category of growers and processors and reduce the number of permits for secure transporter and safety compliance center to 1 in the ordinance?

Thank you for your time and consideration on this matter. I look forward to hearing from you.

Sincerely,

Yingxin Zhou
2565 Sophiea Pkwy
Okemos, MI 48864

Peter Menser

From: Carolyn Sebestyen <555csebestyen@gmail.com>
Sent: Monday, August 19, 2019 11:00 AM
To: Planning Commision (DG)
Subject: Bennett / Hagadorn Mayberry development

Dear Commissioners

The Township's All Neighborhoods meeting tonite regarding developers change in zoning request is an effort seek compromise on the overwhelming voiced concerns the community has already expressed is insulting. Neither the Township nor the developer can steer around the negative impacts increasing the number of houses the zoning variance would cause. The property is zoned. Develope it or don't within the current zoning parameters. Traffic problems, school over crowding, negative environmental impact on wetlands and overwhelming drainage system are NOT issues the Planning Commission should kick down the road to the School Board, Road Commission and Drain Commission to fix as the above problems become a reality once variance is granted.

Go sit at Bennett/Hullet round-about the next week for the beginning of school year. Imagine what 250 plus households will have....do the math yourself, cars per household, number kids per household, wastewater, runoff. Go park in one of those higher density communities on a busy weekend. I have. Its not pretty. Lastly, we already locked in the school mileage and road milaege for foreseeable future. Did either of those get to take this increase into consideration? My research says no they did not. So please, do not allow the variance. Lots of issues, no ready solutions, no more mileage funds. The developer knew what the zoning was/is. Vote no.

Peter Menser

From: JULIA WHITE <juliawhite2@me.com>
Sent: Monday, August 19, 2019 5:01 PM
To: Planning Commision (DG)
Subject: Object to LaFontaine's Special Use Permit

Dear Planning commission,

I would not support LaFontaine's Special Use Permit and urge you to not allow it. We would do better to use other areas already in pavement and save our natural areas.

Please consider preserving natural areas! Thank You.

Julia White
Seminile Drive
Okemos

Peter Menser

From: Evgenia Barysheva <evgeniya.n.barysheva@gmail.com>
Sent: Tuesday, August 20, 2019 9:56 AM
To: Peter Menser; Planning Commision (DG)
Subject: SUP Amendment #19-74011 Appeal Follow-Up

Dear Peter and Members of the Planning Commission:

First of all, thank you for allowing us to present our appeal (against approval of SUP amendment #19-74011) at the busy meeting on August 12, 2019. As a follow-up on that discussion, we would like to inquire about the status of our appeal.

Second, based on (1) responses obtained from the staff of the Community Planning and Development Department and (2) the public hearing discussions, we would like to extend our concerns and questions and request another opportunity to speak at the next meeting on August 26, 2019.

Third, we would also like to know who/when/how will be addressing the Code violations by the Radmoor School with regards to the setback requirements and lack of proper landscaping buffering on the southern line of our property at 2767 Mt. Hope Rd. Is any additional action required from us at this point?

Last, we noticed that the School's playground was not mentioned by the amendments that were sent to us after we had requested to see all previous amendments. Could you, please, share with us this specific amendment that approved addition of the playground?

Best regards,
Sergey Baryshev and Evgenia Barysheva

Peter Menser

From: championwoodsokemos@gmail.com
Sent: Sunday, August 11, 2019 3:56 PM
To: Peter Menser; Mark Kieselbach
Cc: Board; Planning Commision (DG)
Subject: Yield plans associated with rezoning 19060

Hi Peter and Mark,

In the 8/8 Township Board meeting, the applicant stated he could supply your office with their preliminary yield plans for the current zoning and the proposed zoning. To properly prepare for the mediation proposed by the Township Board, would you please make these yield plans available to the Champion Woods Condominium Association and all other interested parties in this rezoning request? Also, if any new materials regarding this rezoning are submitted by the applicant, we would appreciate a prompt notification of the submissions (if necessary, I can set up a daily email query to request such information).

Thanks,

Eric Torng
Champion Woods CA President
4138 Benca Way
Okemos, MI 48864
(517) 944-5179
Pronouns: he/him/his

Peter Menser

From: Yingxin Zhou <zhou0824@gmail.com>
Sent: Saturday, August 10, 2019 4:02 AM
To: Peter Menser; Planning Commision (DG)
Cc: Eric Torng; McCole, Daniel; Carla Galligan; Kathy & Larry McCurdy
Subject: Re: Rezoning #19060- new company name

Sorry I forgot to mention that the applicant on the applicant form is still Bennett road holding LLC. Could you please ask the applicant to update the application form? Also the size of the project site of 96.74 acres and the current zoning class (RAAA and RR) in the application form is wrong. Could you please ask the applicant to fix these all together?

Thank you, Yingxin Zhou

On Sat, Aug 10, 2019 at 3:55 PM Yingxin Zhou <zhou0824@gmail.com> wrote:

Hi, Peter,

I looked at the parcel 33-02-02-29-300-025 at BS &A online and noticed the owner was changed from Bennett road holding LLC to Okemos land investment LLC on 7/16/19. Please see the attached file. Do you know how the new entity is related to Mayberry homes? Could you please ask the new owner to sign the rezoning application and also request all land ownership certificates to be included in the new application?

Thank you. I look forward to hearing from you.
Yingxin Zhou

Peter Menser

From: Yingxin Zhou <zhou0824@gmail.com>
Sent: Saturday, August 10, 2019 3:55 AM
To: Peter Menser; Planning Commision (DG)
Cc: Eric Torng; McCole, Daniel; Carla Galligan; Kathy & Larry McCurdy
Subject: Rezoning #19060- new company name
Attachments: Parcel Number - 33-02-02-29-300-025 Meridian Charter Township BS&A Online.pdf

Hi, Peter,

I looked at the parcel 33-02-02-29-300-025 at BS &A online and noticed the owner was changed from Bennett road holding LLC to Okemos land investment LLC on 7/16/19. Please see the attached file. Do you know how the new entity is related to Mayberry homes? Could you please ask the new owner to sign the rezoning application and also request all land ownership certificates to be included in the new application?

Thank you. I look forward to hearing from you.
Yingxin Zhou

Property Owner: OKEMOS LAND INVESTMENT LLC

Summary Information

- > Assessed Value: \$78,500 | Taxable Value: \$78,500
- > Property Tax information found
- > 1 Special Assessment found
- > 1 Building Department records found



Item 1 of 2 1 Image / 1 Sketch

Parcel is Vacant

Owner and Taxpayer Information

Owner	OKEMOS LAND INVESTMENT LLC 1650 KENDALE BLVD STE 200 EAST LANSING, MI 48823	Taxpayer	SEE OWNER INFORMATION
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General Information for Tax Year 2019

Property Class	COMMERCIAL VACANT	Unit	02 MERIDIAN TOWNSHIP
School District	OKEMOS	Assessed Value	\$78,500
MG	<i>No Data to Display</i>	Taxable Value	\$78,500
APPRAISAL	<i>Not Available</i>	State Equalized Value	\$78,500
C. AREA	<i>Not Available</i>	Date of Last Name Change	08/06/2019
INFLUENCE	<i>Not Available</i>	Notes	<i>Not Available</i>
Historical District	<i>Not Available</i>	Census Block Group	<i>No Data to Display</i>
PROBLEMS	<i>Not Available</i>	Exemption	<i>No Data to Display</i>

Principal Residence Exemption Information

Homestead Date *No Data to Display*

Principal Residence Exemption	June 1st	Final
2020	0.0000 %	-
2019	0.0000 %	0.0000 %

Previous Year Information

Year	MBOR Assessed	Final SEV	Final Taxable
2018	\$78,500	\$78,500	\$78,500
2017	\$78,500	\$78,500	\$78,500
2016	\$78,500	\$78,500	\$78,500

Land Information

Zoning Code	RAAA	Total Acres	30.191
Land Value	\$157,000	Land Improvements	\$0
Renaissance Zone	No	Renaissance Zone Expiration Date	<i>No Data to Display</i>
ECF Neighborhood	2035 OKEMOS DOWNTOWN/HAGADORN/OUTLYING OFFICE	Mortgage Code	<i>No Data to Display</i>
Lot Dimensions/Comments	<i>No Data to Display</i>	Neighborhood Enterprise Zone	No

Lot(s)	Frontage	Depth
No lots found.		
Total Frontage: 0.00 ft		Average Depth: 0.00 ft

Legal Description

(M 29-15) PART OF E 1/2 OF SW 1/4 OF SEC 29 T4N R1W DESC AS: COM AT S 1/4 COR OF SEC 29 - W ALNG S SEC LN 200 FT TO POB - W 259.18 FT - N 00D 13' 20" W PLL WITH W 1/8 LN OF SEC 230 FT - N 63D 28' 47" W 223.95 FT - W 200 FT - N 00D 13' 20" W 50 FT - N 45D 06' 39" W 283.39 FT - N 00D 13' 20" W 973.57 FT - N 45 D 02' 06" E 134.42 FT - S 89 D 57' 54" E 962.26 FT TO NS 1/4 LN - S 00D 16' 21" E ALNG NS 1/4 LN 1147.98 FT - S 79D 14' 28" W 250.38 FT - S 49D 08' 47" W 114 FT - S 30D 18' 55" E 175.77 FT - ALNG CURVE TO RT 174.36 FT, RAD OF 333 FT, CHD BRG S 15D 18' 55" E 172.37 FT - S 00D 18' 55" E 60.77FT TO POB EXC COM AT S 1/4 COR OF SEC 29 - W 200 FT ALNG S SEC LN - N 19D 46' 54" W 259.83 FT - N 40D 01' 43" W 147.79 FT - N 19D 32' 36" W 143.97 FT - N 00D 56' 32" E 112 FT TO POB - N 00D 56' 32" E 320 FT - E 275 FT - S 00D 56' 32" W 320 FT - W 275 FT TO POB 30.191 A

Sale History

Sale Date	Sale Price	Instrument	Grantor	Grantee	Terms of Sale	Liber/Page	Comments
07/16/2019	\$1.00	QC	BENNETT ROAD HOLDING LLC	OKEMOS LAND INVESTMENT LLC	NOT USED ECF	2019-024479	
08/21/2014	\$1.00	QC	EL HOLDING CO LLC	BENNETT ROAD HOLDING LLC	NOT USED ECF	2014-033591	
10/31/2013	\$213,492.00	WD	ICD PROPERTIES LLC	EL HOLDING CO LLC	LAND ONLY	2013-051852	SALE \$ FROM PTA
01/22/2008	\$1.00	WD	GTC INVESTMENTS CO	ICD PROPERTIES LLC	NOT USED ECF		

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To: Planning Commission

From: Peter Menser, Principal Planner
Justin Quagliata, Assistant Planner

Date: August 23, 2019

Re: Special Use Permit #19111 (Woodward Limited Dividend Housing Association), construct a 49-unit townhouse and apartment complex comprised of four buildings totaling 52,861 square feet on the north 4.37 acres of an undeveloped parcel identified as Parcel Number 17-377-031.

The Planning Commission last discussed Special Use Permit #19111 at its meeting on August 12, 2019. At the public hearing on July 22, 2019 the Planning Commission indicated support for the project, but expressed concern for the number of variances that would have been required to facilitate the development. At the last meeting a revised site plan that included changes to the site layout was reviewed by the Planning Commission. The number of variances required was reduced from 10 to three. Since the last meeting the applicant has submitted a revised site plan that reduced the number of required variances from three to two. Following is a summary of the changes the applicant has made to the project since the originally submitted site plan.

Setback between Buildings B-2 and B-3

Based on the initial site plan a variance was required for the setback between Buildings B-2 and B-3. With the revised site plan the location and orientation of buildings have shifted to meet all setbacks between buildings, so a variance is not necessary.

Maximum building dimension

No single building or connected building may exceed 200 feet in any one dimension. The revised site plan shows Building C-4 was reduced in length from 205 feet to 200 feet, so the previously identified five foot variance is no longer required. The length of Buildings A-1, B-2, and B-3 remained the same. At 237 feet in length, Buildings B-2 and B-3 require a 37 foot variance for the maximum building dimension. Based on the reduced size of Building C-4 the total size for all buildings in the development decreased from 53,353 square feet to 52,861 square feet in size, a 492 square foot reduction.

Building setbacks from parking areas

Based on the building layout shown on the previous site plans variances would have been required from the 25 foot setback from the entrance of a multiple family structure to a parking area and the 20 foot setback from the wall of a multiple family structure to a parking area. Based on the revised site plan all buildings in the development meet both setbacks, therefore the previously identified variances are no longer required.

**Special Use Permit #19111 (Woodward Limited Dividend Housing Association)
Planning Commission (August 26, 2019)
Page 2**

Size of parking spaces

The size of the parking spaces were reduced from 200 square feet (10 feet by 20 feet) in size to 180 square feet (9 feet by 20 feet) in size. The multiple family zoning district and parking ordinance (Section 86-756(3)) state a minimum of 200 square feet must be provided for each vehicle parking space located within a multiple family residential development. A 20 square foot variance for the size of each parking space is required.

Parking setback from a nonresidential zoning district

The expansion capacity parking area east of Building A-1 was moved north to meet the 15 foot parking setback from the south property line, which is also the C-2 (Commercial) zoning district boundary. By relocating the parking area the applicant eliminated the previously necessary variance for the parking setback. The number of parking spaces provided in the project was reduced from 99 to 98. A minimum of 97 parking spaces are required to serve the site.

With the revised site layout a 20 foot variance from the 40 foot parking setback from the residential zoning district to the east is no longer required.

Buffering along the north and east property lines

The original site plan proposed no screening along the north and east property lines. The previous site plan showed a four foot tall landscape earth berm along the north and east property lines. Discussion at the last meeting included challenges of maintaining landscaping planted on a berm. The revised site plan shows a double row of conifer trees along the north and east property lines for screening.

With the revised site plan building coverage is at 15.45 percent and 51.95 percent of the site is open space. The maximum building coverage allowed in the RC (Multiple Family) zoning district is 35 percent and the minimum open space required is 35 percent.

The following list summarizes the required variances based on the revised site plan:

- 20 square foot variance for the size of each parking space
- 37 foot variance for the maximum dimension of Buildings B-2 and B-3

Planning Commission Options

The Planning Commission may approve, approve with conditions, or deny the special use permit. A resolution to approve the special use permit with conditions is provided.

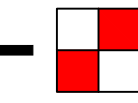
- **Motion to adopt the resolution to approve Special Use Permit #19111 with conditions.**

**Special Use Permit #19111 (Woodward Limited Dividend Housing Association)
Planning Commission (August 26, 2019)
Page 3**

Attachments

1. Revised site plan prepared by Monument Engineering Group Associates, Inc. dated August 7, 2019 (revision date August 21, 2019) and received by the Township on August 21, 2019.
2. Revised building elevations and floor plans prepared by PCI Design Group, Inc. dated August 21, 2019 and received by the Township on August 22, 2019.
3. Resolution to approve.

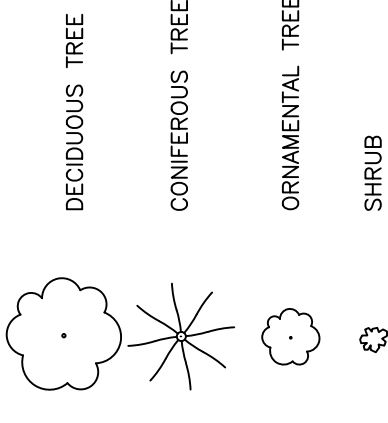
G:\Community Planning & Development\Planning\SPECIAL USE PERMITS (SUP)\2019\SUP 19111 (Woodward Limited Dividend Housing Association)\SUP 19111.pc3.doc



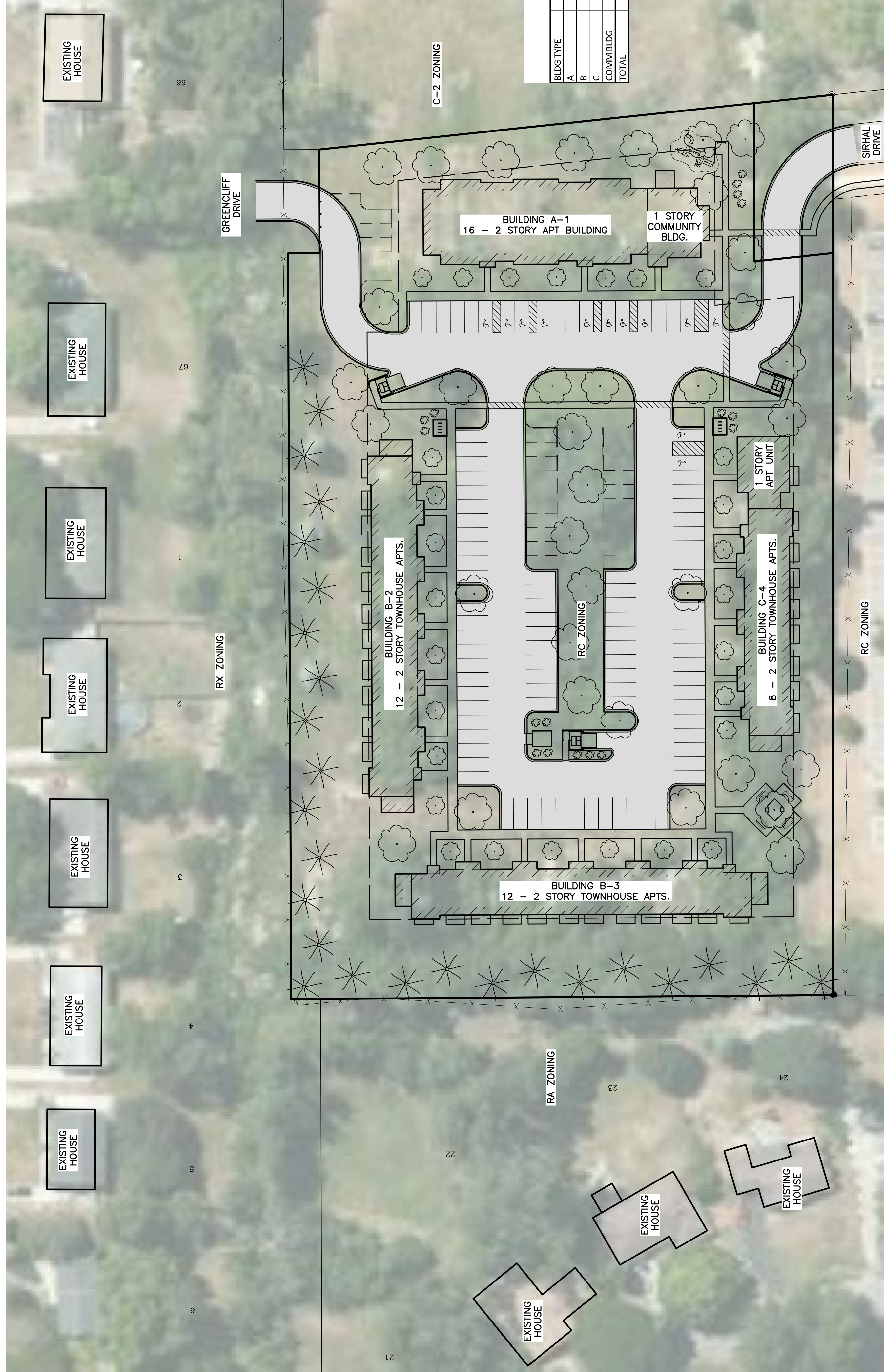
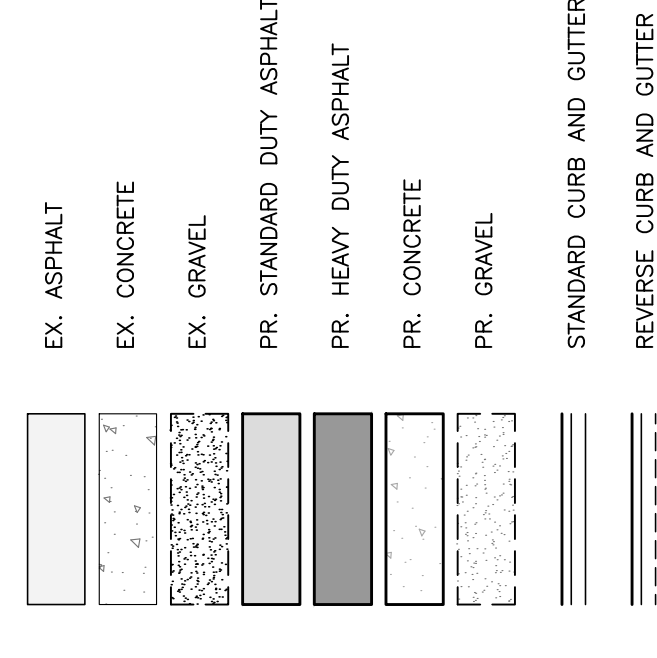
WOODWARD WAY
SITE PLAN OVERVIEW



LANDSCAPE LEGEND



PAVEMENT LEGEND



SITE PLAN OVERVIEW
WOODWARD WAY
SIRHAL & GREENCLIFF DRIVES
MERIDIAN TOWNSHIP, MI

CLIENT :
WODA COOPER
COMPANIES, INC.
500 S FRONT STREET
SUITE 975
COLUMBUS, OH 43215

ENGINEERS - SURVEYORS
CONSULTANTS - LAND PLANNERS
MEGA
Montreal Engineering Group Associates, Inc.
298 VETERANS DRIVE
MICHIGAN 48836
(OFFICE) 517-223-3512
MONUMENTENGINEERING.COM
SERVICE DISABLED VETERAN OWNED
SMALL BUSINESS (SDVOB)

ZONING INFORMATION

THE ZONING INFORMATION IS TAKEN FROM MERIDIAN TOWNSHIP ZONING ORDINANCE DATED: JANUARY 25, 2019

SUBJECT PARCEL ZONING CLASSIFICATION:
RC

BUILDING SETBACKS:	REQUIRED	PROVIDED
FRONT (WEST):	25'	25'
SIDE (NORTH):	50'	50'
SIDE (SOUTH):	25'	25'
REAR (EAST):	50'	50'

ADJACENT ZONING:
NORTH: RA
SOUTH: C-2
EAST: RX
WEST: RC

NOTES

1. SIGN TO BE LOCATED AND MEET ALL TOWNSHIP STANDARDS
2. SCREENING SHALL BE PROVIDED FOR ALL MECHANICAL EQUIPMENT.
3. PARKING LOT ISLANDS SHALL BE FINISHED IN GRASS, GROUND COVER OR MULCH.
4. STORM WATER MANAGEMENT TO MEET ALL TOWNSHIP & STATE STORM WATER MANAGEMENT REQUIREMENTS
5. CONSTRUCTION TYPE TO BE SLAB ON GRADE, WOOD FRAME WITH BRICK VENEER AND VINYL SIDING, I.B.C. CONSTRUCTION TYPE: 5-B.
6. NO EXISTING STRUCTURES ON SITE
7. NO NATURAL WATER FEATURES ON SITE

SITE DEVELOPMENT DATA

BLDG TYPE	NO	1 BR	1 BR/HC	2 BR	2 BR/HC	3 BR	3 BR/HC	TOTAL	REMARKS
A	1	6/6	2/2	5/5	3/3	0	0	16	2 STORY BLDG
B	2	0	0	10/20	0	2/4	0	24	1&2 STORY BLDG
C	1	0	0	6/6	0	2/2	1/1	9	1&2 STORY BLDG
COMM BLDG	1	0	0	0	0	0	0	0	1 STORY BLDG
TOTAL	5	6	2	31	3	6	1	49	

TOTAL UNITS = 49

LAND AREA	190296 SF	4.37 AC
LAND DENSITY	3883.6 SF/UNIT	111.21 UNIT/AC

PARKING	REQ PER UNIT	OFFICE	# OF UNITS/SF	SPACES REQ.
MOTOR VEHICLE	2/UNIT	1/200 SF	49 UNITS/4418 SF	105
BICYCLE	1/10 SPACES	N/A	105	11
REDUCTION	1/2 SPACES	N/A	16	8
TOTAL				98 SPACES
EXPANSION	25%	N/A	97	25 SPACES

BUILDING AREA CALCULATIONS

BLDG	AREA	# OF UNITS
A1	9014 SF	16
B2	7109 SF	12
B3	7109 SF	12
C4	6047 SF	9
MAINT BLDG	120 SF	0
TOTAL	29399 SF	49
PERCENT BLDG COVERAGE	15.45%	
ALLOWABLE BLDG COVERAGE	35%	

IMPERVIOUS SURFACE CALCULATIONS

TOTAL SITE SIZE	190296 SF
BUILDING COVERAGE	29399 SF
SIDEWALKS & PATIOS	12322 SF
PARKING & DRIVES	47037 SF
CURB & GUTTER	2678 SF
TOTAL IMPERVIOUS AREA	91436 SF
PERCENT IMPERVIOUS AREA	48.05%
MAX IMPERVIOUS ALLOWED	70%

MIN OPEN SPACE	51.95%
MIN OPEN SPACE ALLOWED	35%

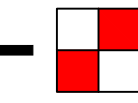
ORIGINAL ISSUE DATE:
8/07/2019

PROJECT NO: 19-022

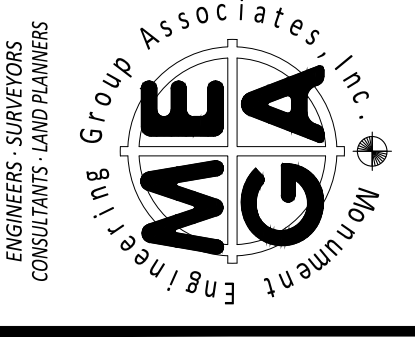
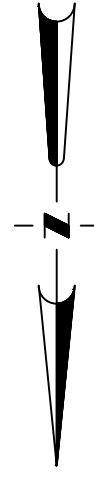
SCALE: 1" = 20'

FIELD:
DRAWN BY:
CHECK BY: AP

S1



WOODWARD WAY
SITE PLAN



298 VETERANS DRIVE
MICHIGAN 48836
OFFICE 517-223-3512
MONUMENTENGINEERING.COM
SERVICE DISABLED VETERAN OWNED
SMALL BUSINESS (SDVOB)

Call MISS DIG
3 full working days before you dig
Michigan's
One-Call
Utility
Notification
Organization
1-800-482-7171
www.missdig.org

CLIENT :
**WODA COOPER
COMPANIES, INC.**
500 S FRONT STREET
SUITE 975
COLUMBUS, OH 43215

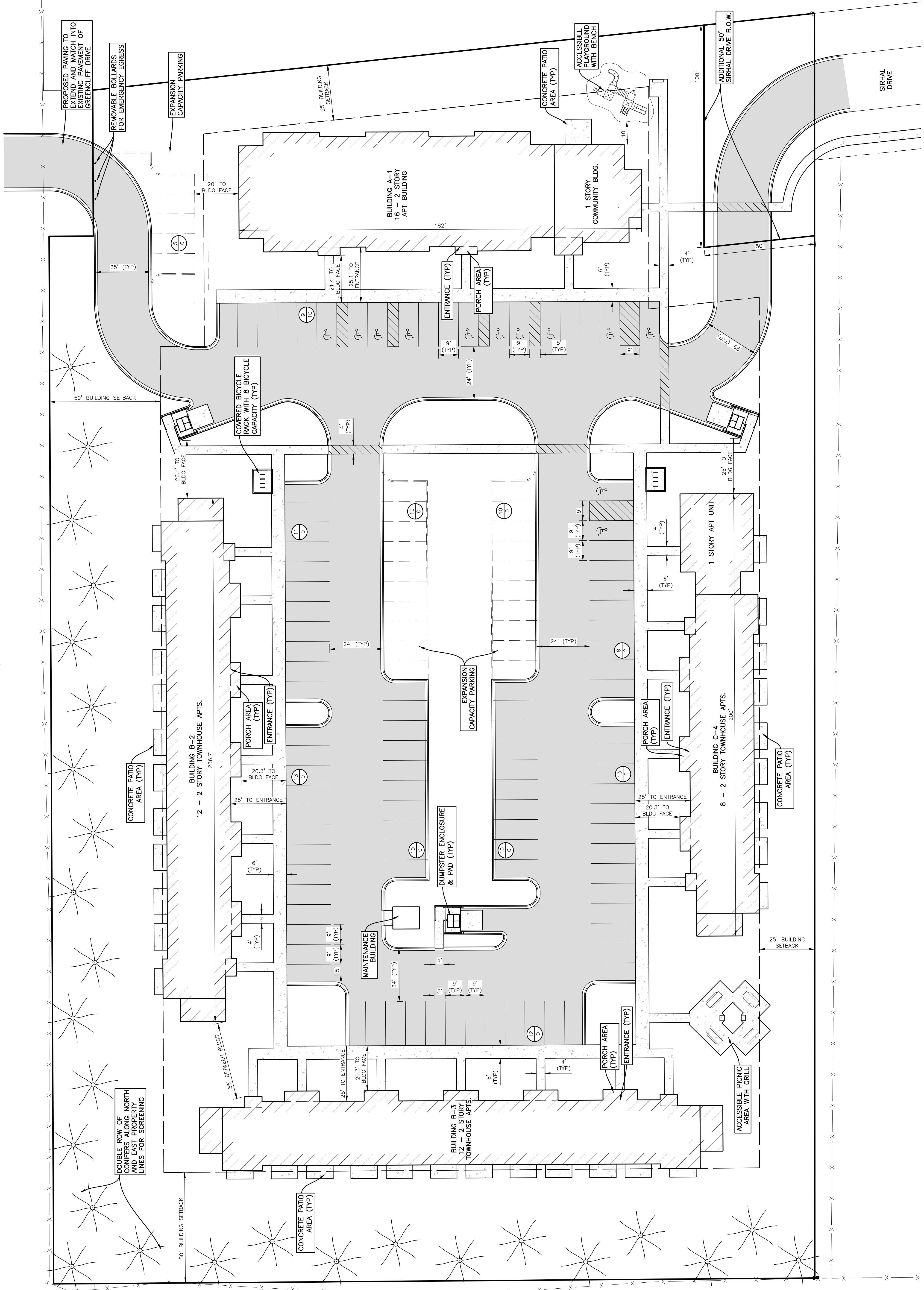
SITE PLAN
WOODWARD WAY
SIRHAL & GREENCLIFF DRIVES
MERIDIAN TOWNSHIP, MI

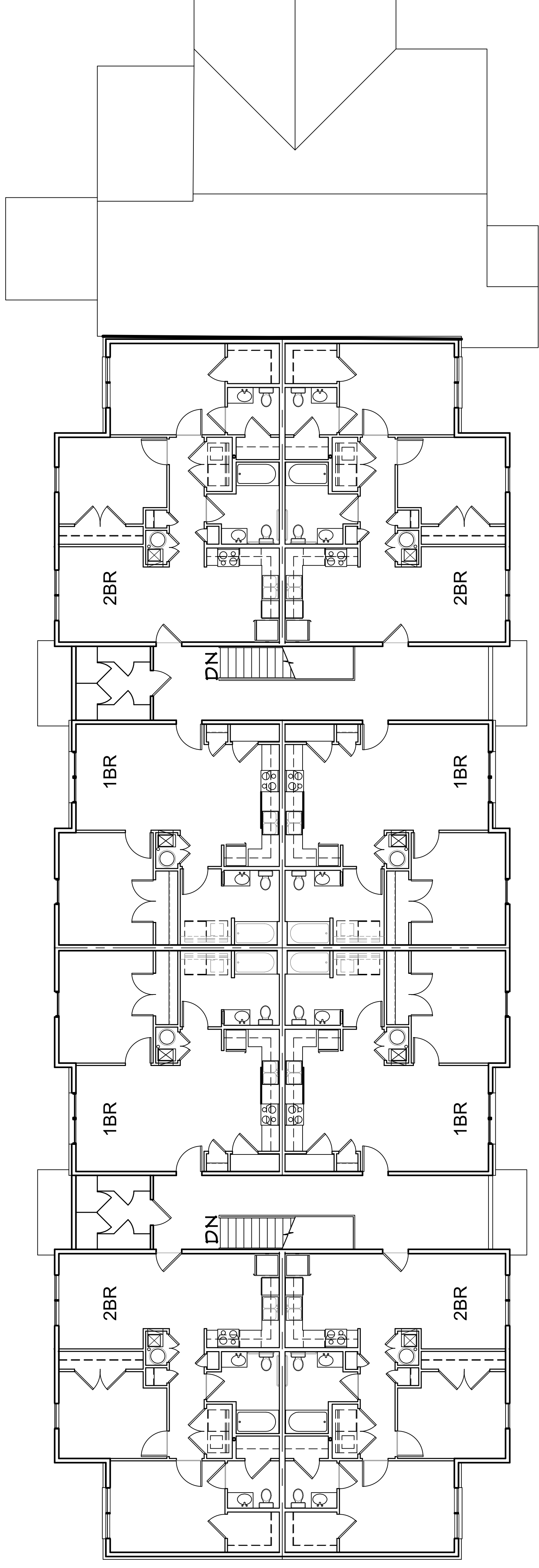
DATE	8/07/2019
PLAN SUBMITTALS/REVISIONS	PRELIMINARY SITE PLAN LAYOUT REISED PRELIM SITE PLAN LAYOUT 8/21/2019
ORIGINAL ISSUE DATE:	8/07/2019

PROJECT NO: 19-022
SCALE: 1" = 20'
0 1/2" 1"

FIELD:
DRAWN BY:
CHECK BY: AP

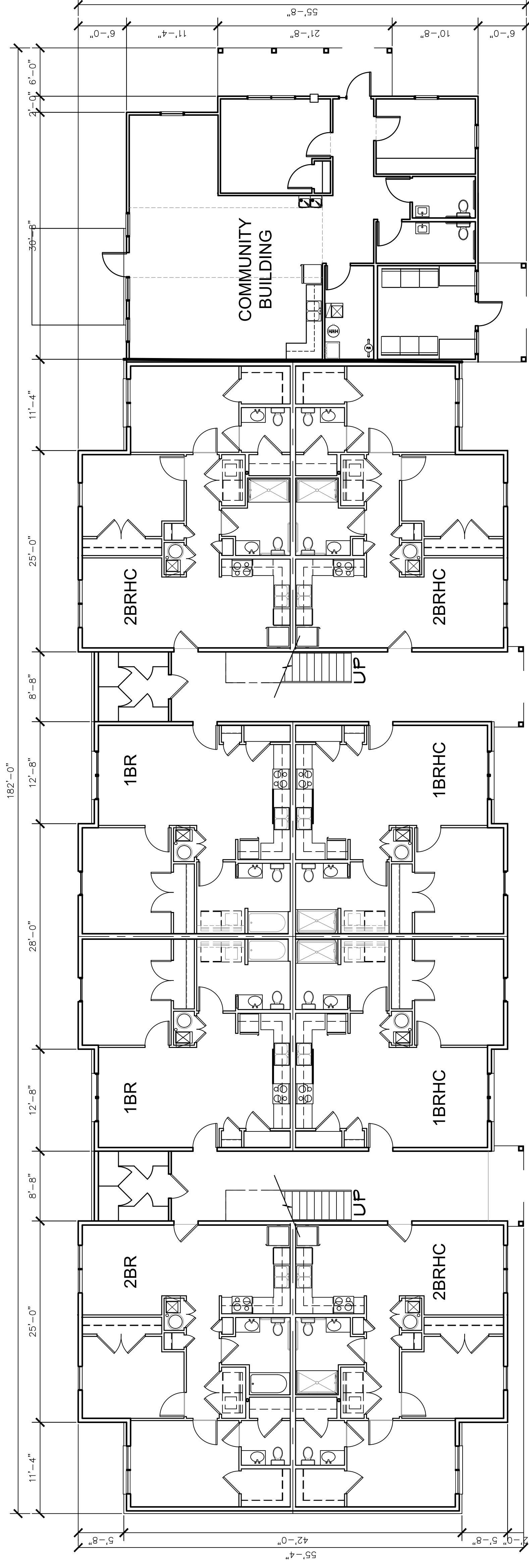
S2





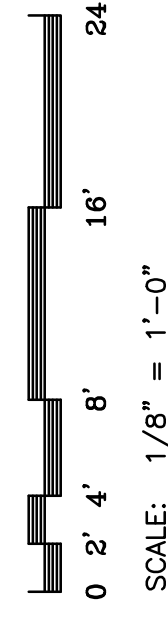
SECOND FLOOR PLAN

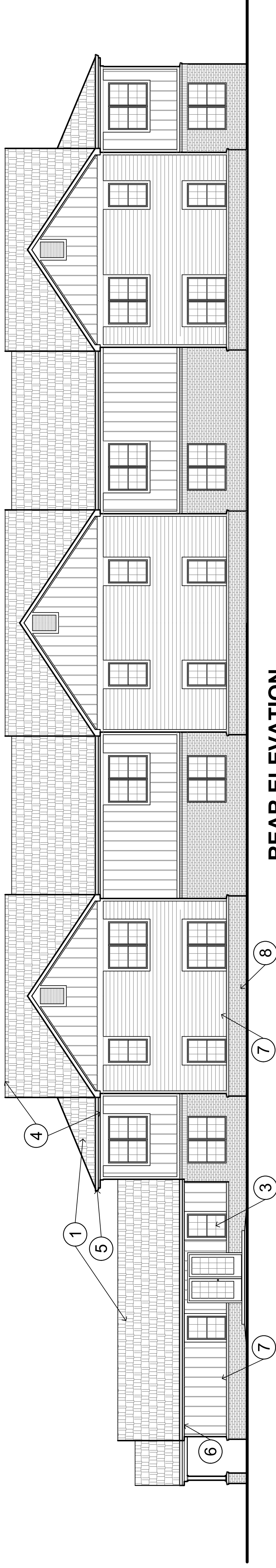
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FIRST FLOOR PLAN

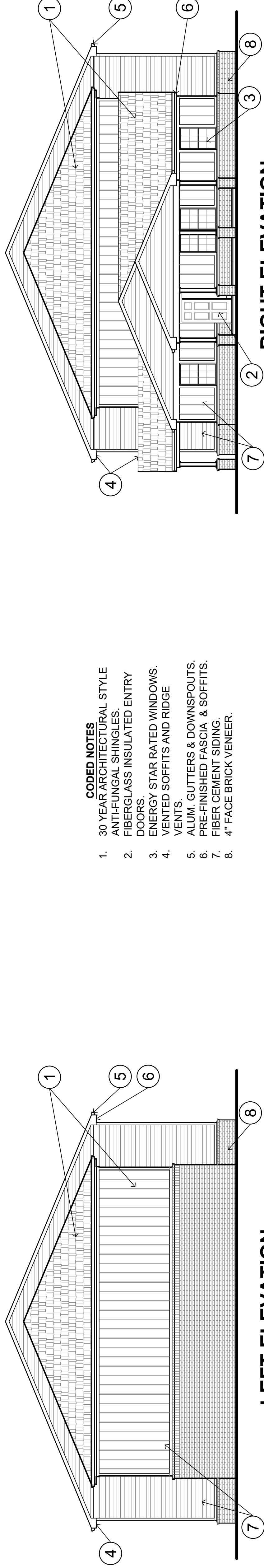
SCALE 1/8" = 1'-0"





REAR ELEVATION

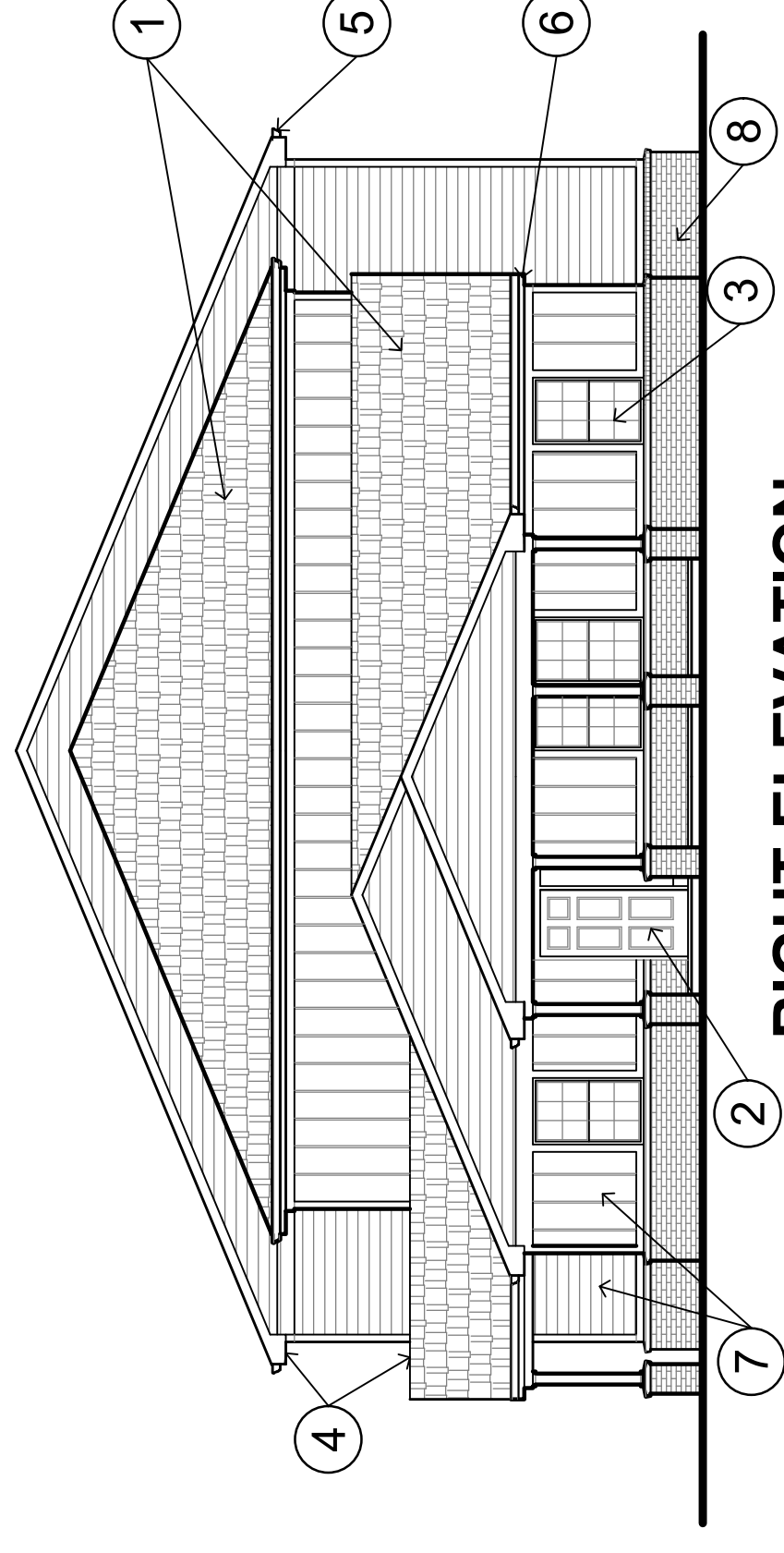
SCALE 1/8" = 1'-0"



LEFT ELEVATION

SCALE 1/8" = 1'-0"

- CODED NOTES**
1. 30 YEAR ARCHITECTURAL STYLE ANTI-FUNGAL SHINGLES.
 2. FIBERGLASS INSULATED ENTRY DOORS.
 3. ENERGY STAR RATED WINDOWS.
 4. VENTED SOFFITS AND RIDGE VENTS.
 5. ALUM. GUTTERS & DOWNSPOUTS.
 6. PRE-FINISHED FASCIA & SOFFITS.
 7. FIBER CEMENT SIDING.
 8. 4" FACE BRICK VENEER.



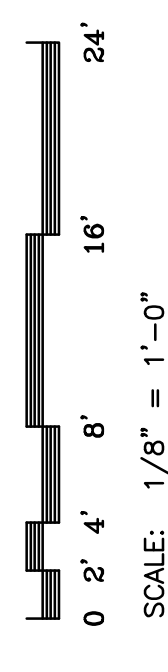
RIGHT ELEVATION

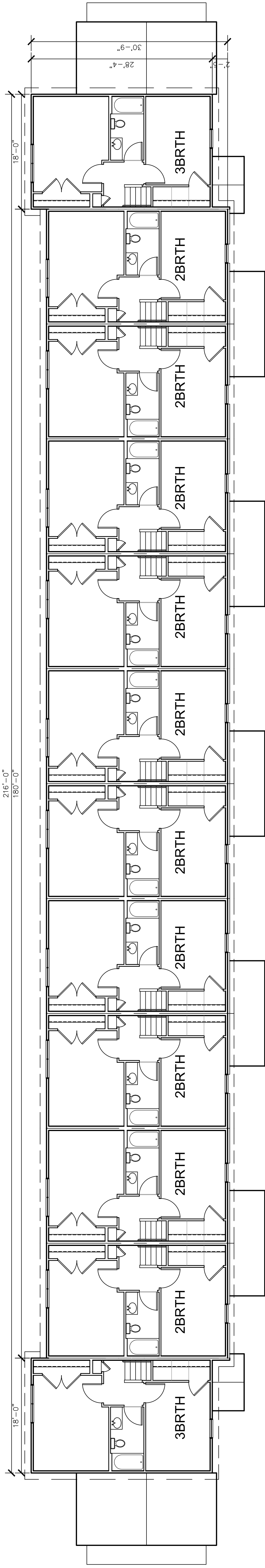
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FRONT ELEVATION

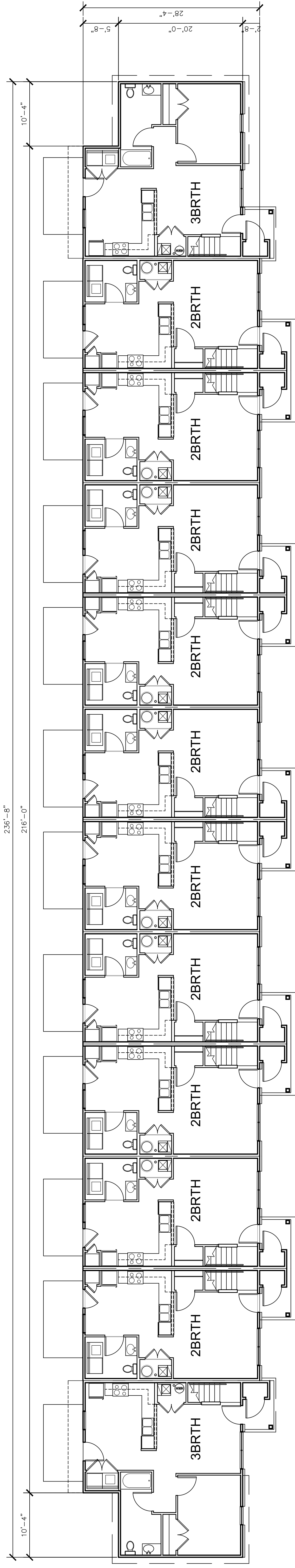
SCALE 1/8" = 1'-0"





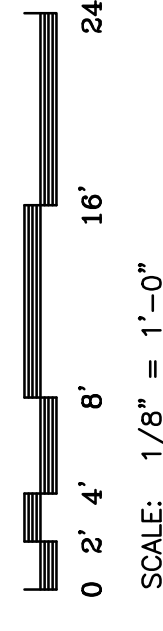
SECOND FLOOR PLAN

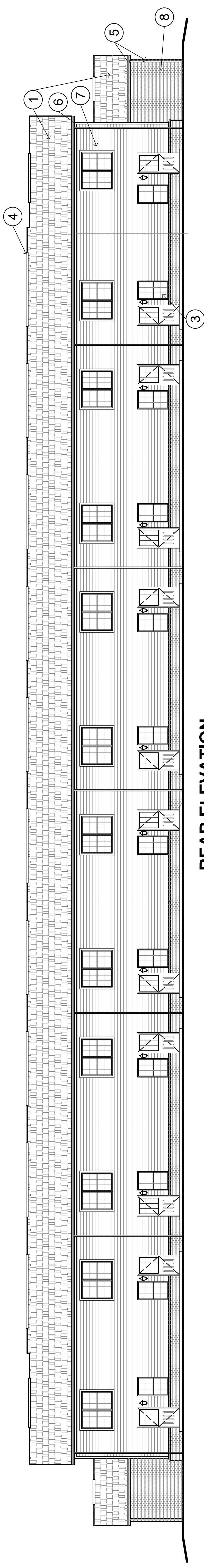
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FIRST FLOOR PLAN

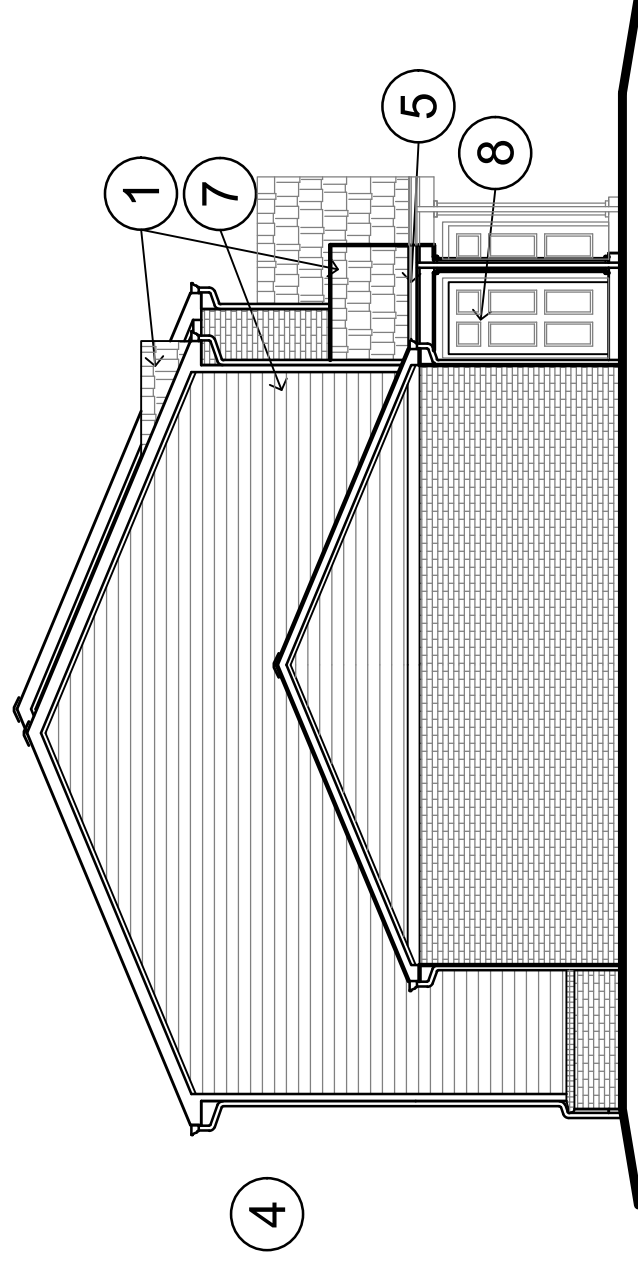
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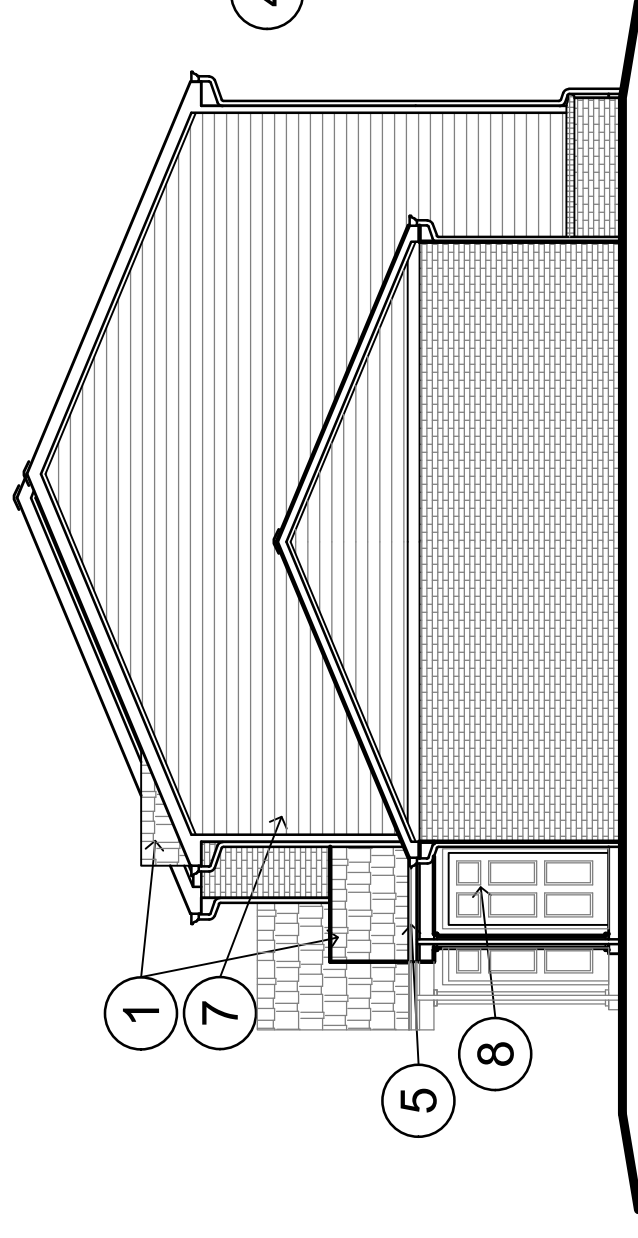
REAR ELEVATION

SCALE 1/8" = 1'-0



LEFT ELEVATION

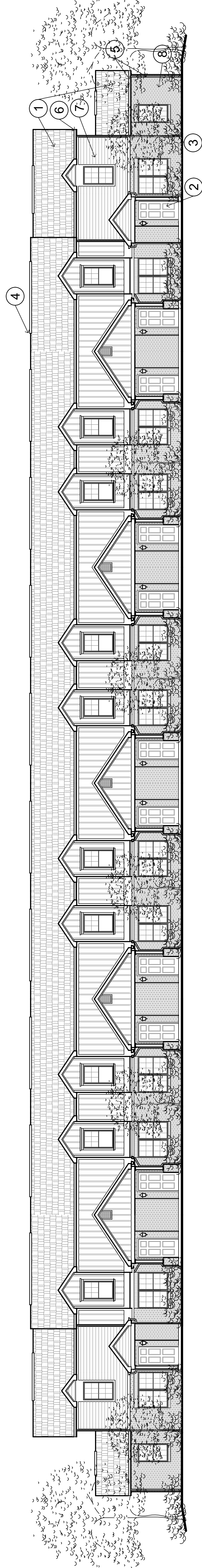
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RIGHT ELEVATION

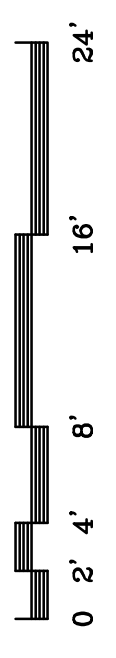
SCALE 1/8" = 1'-0

- CODED NOTES**
1. 30 YEAR ARCHITECTURAL STYLE ANTI-FUNGAL SHINGLES.
 2. FIBERGLASS INSULATED ENTRY DOORS.
 3. ENERGY STAR RATED WINDOWS.
 4. VENTED SOFFITS AND RIDGE VENTS.
 5. ALUM. GUTTERS & DOWNSPOUTS.
 6. PRE-FINISHED FASCIA & SOFFITS.
 7. FIBER CEMENT SIDING.
 8. 4" FACE BRICK VENEER.

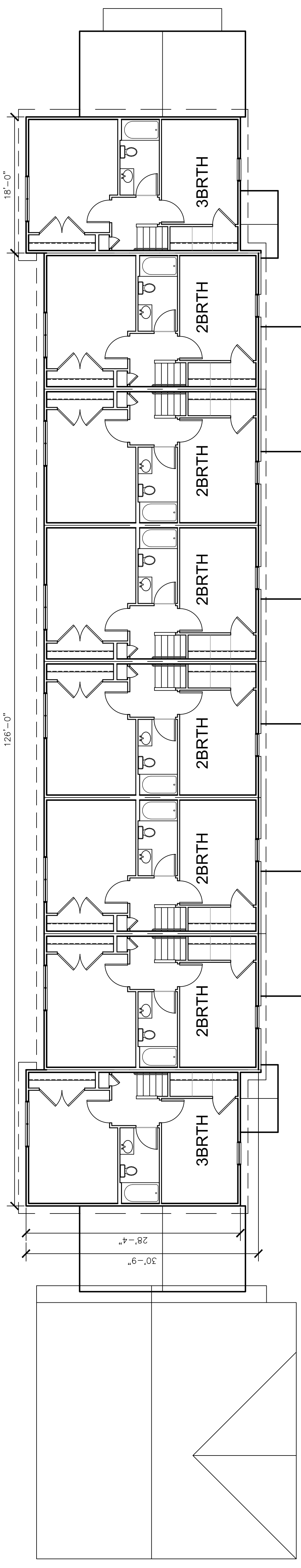


FRONT ELEVATION

SCALE 1/8" = 1'-0

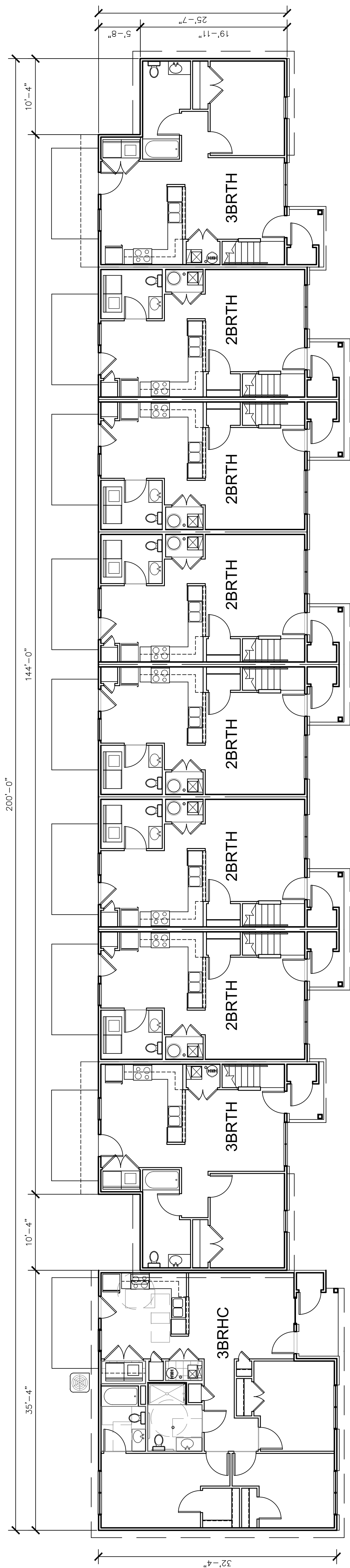


SCALE: 1/8" = 1'-0"



SECOND FLOOR PLAN

SCALE 1/16" = 1'-0"

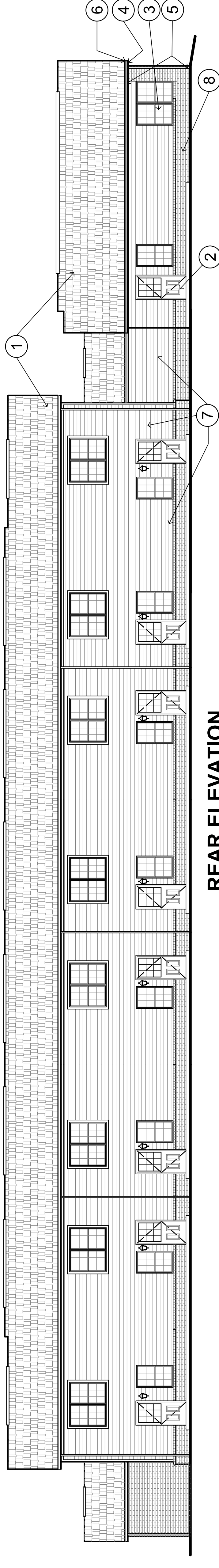


FIRST FLOOR PLAN

SCALE 1/16" = 1'-0"

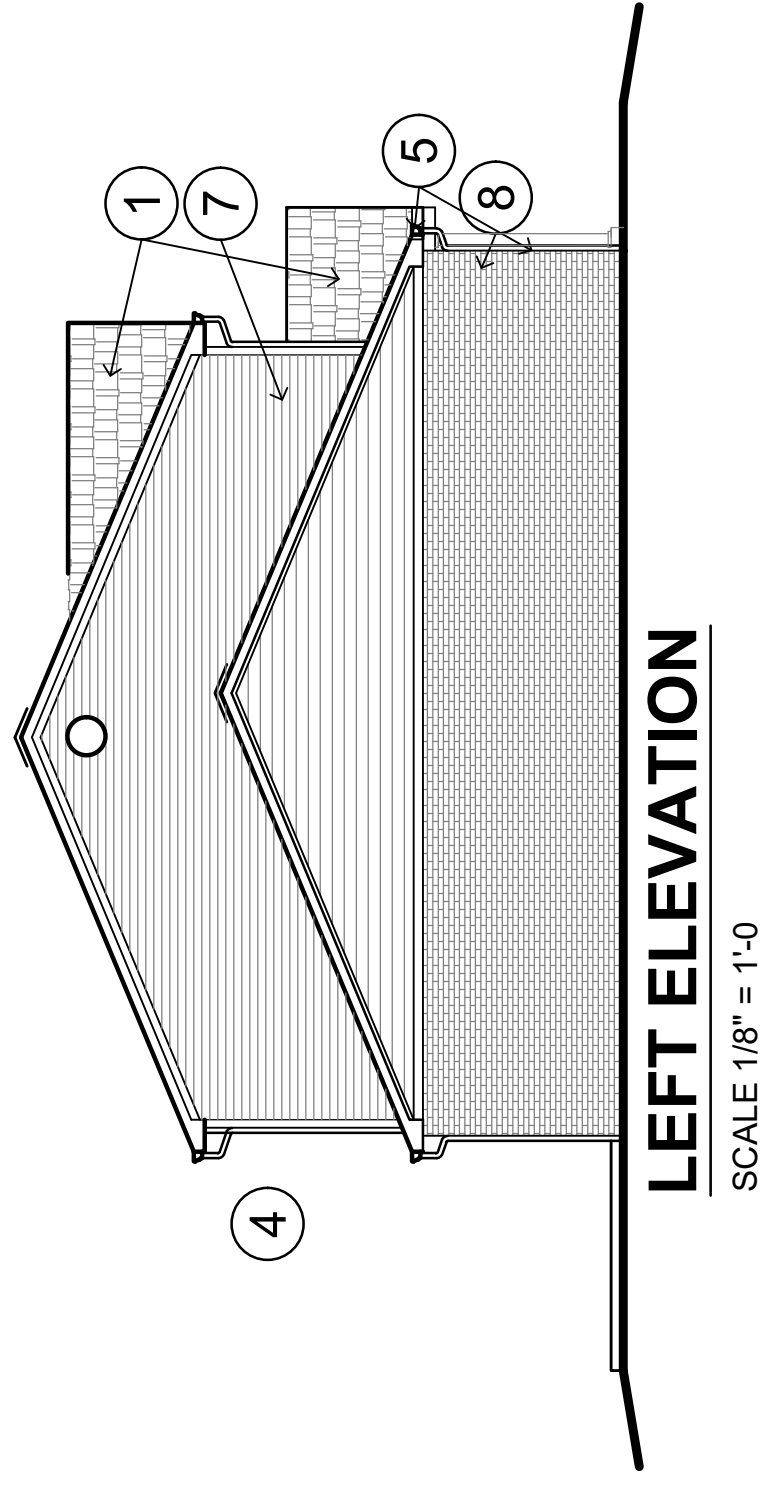


SCALE: 1/8" = 1'-0"



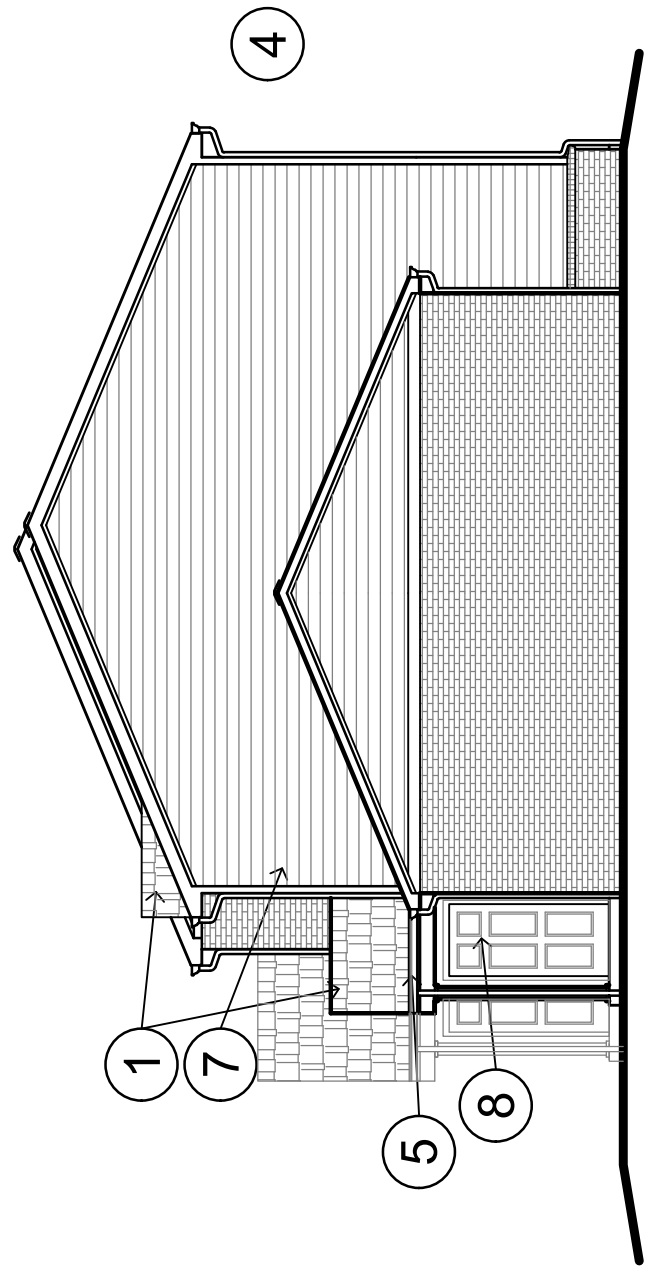
REAR ELEVATION

SCALE 1/8" = 1'-0"



LEFT ELEVATION

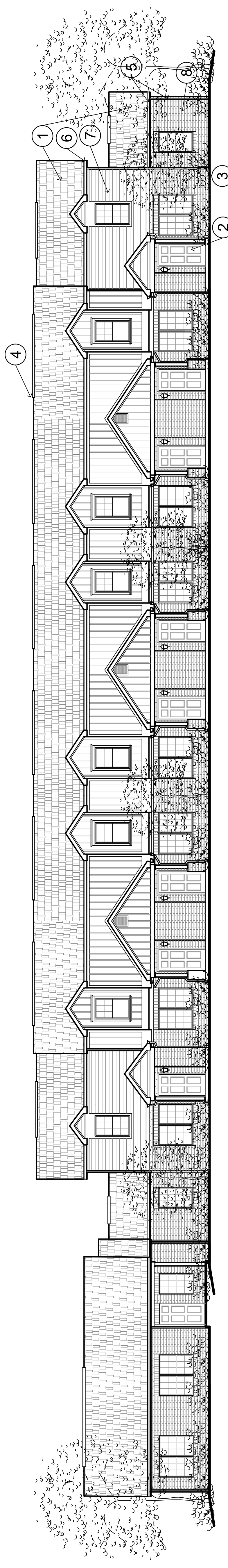
SCALE 1/8" = 1'-0"



RIGHT ELEVATION

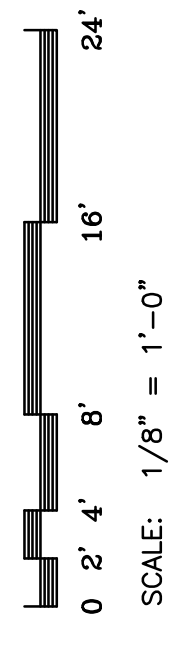
SCALE 1/8" = 1'-0"

- CODED NOTES**
1. 30 YEAR ARCHITECTURAL STYLE ANTI-FUNGAL SHINGLES.
 2. FIBERGLASS INSULATED ENTRY DOORS.
 3. ENERGY STAR RATED WINDOWS.
 4. VENTED SOFFITS AND RIDGE VENTS.
 5. ALUM. GUTTERS & DOWNSPOUTS.
 6. PRE-FINISHED FASCIA & SOFFITS.
 7. FIBER CEMENT SIDING.
 8. 4" FACE BRICK VENEER.



FRONT ELEVATION

SCALE 1/8" = 1'-0"



SCALE: 1/8" = 1'-0"

RESOLUTION TO APPROVE

**Special Use Permit #19111
(Woodward Limited Dividend Housing Association)**

At a regular meeting of the Planning Commission of the Charter Township of Meridian, Ingham County, Michigan, held at the Meridian Municipal Building, in said Township on the 26th day of August, 2019, at 7:00 p.m., Local Time.

PRESENT: _____

ABSENT: _____

The following resolution was offered by _____ and supported by _____.

WHEREAS, Woodward Way Limited Dividend Housing Association requested a special use permit to construct a 49-unit townhouse and apartment complex comprised of four buildings totaling 52,861 square feet on the north 4.37 acres of an undeveloped parcel identified as Parcel Number 17-377-031; and

WHEREAS, the Planning Commission held a public hearing at its regular meeting on July 22, 2019 and has reviewed staff material forwarded under cover memorandums dated July 19, 2019, August 8, 2019, and August 23, 2019; and

WHEREAS, the subject site is appropriately zoned RC (Multiple Family, maximum 14 units per acre), which allows multiple family housing projects with three or more units by special use permit; and

WHEREAS, the proposed housing project is consistent with the conditions established in Rezoning #19010; and

WHEREAS, the proposed housing project meets the design standards of the RC (Multiple Family) zoning district listed in Section 86-376(g) of the Code of Ordinances; and

WHEREAS, the proposed housing project provides adequate setbacks and screening from surrounding uses; and

WHEREAS, municipal water and sanitary sewer is available to serve the subject site; and

WHEREAS, the proposed housing project is consistent with Objective B of Goal 1 of the 2017 Master Plan to ensure new residential developments meet high standards of visual attractiveness, health and safety, and environmental sensitivity; and

WHEREAS, the proposed housing project is consistent with Goal J of the 2019 Township Board Action Plan to explore housing options and alternatives to encourage diverse populations and targeted groups to live in the Township; and

WHEREAS, the proposed housing project is consistent with the general standards for granting a special use permit found in Section 86-126 of the Code of Ordinances.

**Resolution to Approve
SUP #19111 (Woodward Limited Dividend Housing Association)
Page 2**

NOW THEREFORE, BE IT RESOLVED THE PLANNING COMMISSION OF THE CHARTER TOWNSHIP OF MERIDIAN hereby approves Special Use Permit #19111 to construct a 49-unit townhouse and apartment complex on the north 4.37 acres of an undeveloped parcel identified by Parcel Number 17-377-031, subject to the following conditions:

1. Approval is in accordance with the site plan prepared by Monument Engineering Group Associates, Inc. dated August 7, 2019 (Revision Date August 21, 2019) and received by the Township on August 22, 2019.
2. Approval is in accordance with the building elevations and floor plans prepared by PCI Design Group, Inc. dated August 21, 2019 and received by the Township on August 22, 2019.
3. Approval of Special Use Permit #19111 is contingent on the approval of Special Use Permit #19121 by the Township Board.
4. Approval of the special use permit is subject to all voluntary conditions placed on Rezoning #19010 by the applicant.
5. The applicant shall apply for a land division to split the project site from the parent parcel prior to submitting for site plan review.
6. The applicant shall construct a five-foot wide concrete sidewalk to connect to the existing sidewalk along the north side of Sirhal Drive.
7. Site plan review approval is required to construct the housing project. The final site plan, building elevations, and landscape plan shall be subject to the approval of the Director of Community Planning and Development.
8. The applicant shall obtain all necessary permits, licenses, and approvals from the Ingham County Road Department, Ingham County Drain Commissioner, Michigan Department of Environment, Great Lakes, and Energy, and the Township, as applicable. Copies of all permits, licenses, and approval letters shall be submitted to the Department of Community Planning and Development.
9. Utility, grading, and storm drainage plans for the site are subject to the approval of the Director of Public Works and Engineering and Ingham County Drain Commissioner and shall be completed in accordance with the Township Engineering Design and Construction Standards.
10. No grading or land clearing shall take place on the site until the site plan has been approved by the Director of Community Planning and Development and grading and soil erosion and sedimentation control (SESC) permits have been issued for the project.
11. All utility service distribution lines shall be installed underground.
12. Any future building additions or revisions to the site layout will require amendments to Special Use Permit #19111.
13. The final design of the trash and recycling facilities and related enclosures shall be subject to the approval of the Director of Community Planning and Development.



To: Planning Commission

From: Peter Menser, Principal Planner
Justin Quagliata, Assistant Planner

Date: August 23, 2019

Re: Special Use Permit #19121 (Woodward Limited Dividend Housing Association), construct a group of buildings greater than 25,000 square feet in size on an undeveloped parcel identified as Parcel Number 17-377-031.

The public hearing for Special Use Permit #19121 was held at the July 22, 2019 Planning Commission meeting. Since the public hearing the applicant has submitted a revised site plan that includes several changes. The proposed 49-unit townhouse and apartment complex comprised of four buildings decreased in size from 53,353 square feet to 52,861 square feet in size, a 492 square foot reduction. The staff report for SUP #19111 should be referenced for a more complete overview of the revised site plan.

At the last meeting the Planning Commission agreed to consider a resolution to recommend approval of the special use permit at its next meeting.

Planning Commission Options

The Planning Commission may recommend approval, approval with conditions, or denial of the proposed special use permit. A resolution to recommend approval of the special use permit to the Township Board is provided.

- **Motion to adopt the resolution recommending approval of Special Use Permit #19121.**

Attachment

1. Resolution to recommend approval.

G:\Community Planning & Development\Planning\SPECIAL USE PERMITS (SUP)\2019\SUP 19121 (Woodward Limited Dividend Housing Association)\SUP 19121.pc3.doc

RESOLUTION TO RECOMMEND APPROVAL

**Special Use Permit #19121
(Woodward Limited Dividend Housing Association)**

RESOLUTION

At a regular meeting of the Planning Commission of the Charter Township of Meridian, Ingham County, Michigan, held at the Meridian Municipal Building, in said Township on the 26th day of August, 2019, at 7:00 p.m., Local Time.

PRESENT: _____

ABSENT: _____

The following resolution was offered by _____ and supported by _____.

WHEREAS, Woodward Limited Dividend Housing Association has submitted a request to construct a group of buildings greater than 25,000 square feet in size on the north 4.37 acres of Parcel Number 17-377-031; and

WHEREAS, the proposed project is a 49-unit townhouse and apartment complex comprised of four buildings totaling 52,861 square feet in size; and

WHEREAS, a special use permit is required for constructing a group of buildings totaling more than 25,000 square feet in gross floor area; and

WHEREAS, the Planning Commission held a public hearing at its regular meeting on July 22, 2019 and has reviewed staff material forwarded under cover memorandums dated July 19, 2019, August 8, 2019, and August 23, 2019; and

WHEREAS, the subject site is appropriately zoned RC (Multiple Family), which permits the construction of a group of buildings greater than 25,000 square feet in floor area by special use permit; and

WHEREAS, the proposed housing project is consistent with the conditions established in Rezoning #19010; and

WHEREAS, the proposed housing project meets the design standards of the RC (Multiple Family) zoning district listed in Section 86-376(g) of the Code of Ordinances; and

WHEREAS, the proposed project is consistent with the general standards for granting a special use permit found in Section 86-126 of the Code of Ordinances; and

WHEREAS, public water and sanitary sewer services serve the subject site.

NOW THEREFORE, BE IT RESOLVED THE PLANNING COMMISSION OF THE CHARTER TOWNSHIP OF MERIDIAN hereby recommends approval of Special Use Permit #19121, subject to the following conditions:

**Resolution to Recommend Approval
SUP #19121 (Woodward Limited Dividend Housing Association)**

Page 2

1. Approval is in accordance with the site plan prepared by prepared by Monument Engineering Group Associates, Inc. dated August 7, 2019 (revision date August 21, 2019) and received by the Township on August 22, 2019.
2. Approval of the special use permit is subject to all conditions placed on Special Use Permit #19111 by the Planning Commission.
3. Any future building additions will require an amendment to the special use permit.

ADOPTED: YEAS: _____

NAYS: _____

STATE OF MICHIGAN)

) ss

COUNTY OF INGHAM)

I, the undersigned, the duly qualified and acting Chair of the Planning Commission of the Charter Township Meridian, Ingham County, Michigan, DO HEREBY CERTIFY that the foregoing is a true and a complete copy of a resolution adopted at a regular meeting of the Planning Commission on the 26th day of August, 2019.

John Scott-Craig
Planning Commission Chair



To: Planning Commission

From: Peter Menser, Principal Planner

Justin Quagliata, Assistant Planner

Date: August 23, 2019

Re: Special Use Permit #19101 (LaFontaine Chrysler Dodge Jeep & Ram of Okemos), construct a new car dealership at 1510 and 1560 Grand River Avenue.

The public hearing for Special Use Permit (SUP) #19101 was held at the August 12, 2019 Planning Commission meeting. Since the meeting the applicant has submitted a response to the nine special use permit review criteria (attached). During the public hearing the Planning Commission discussed mirroring the conditions of approval placed on SUP #18081 for the new car dealership at the northeast corner of Grand River Avenue and Powell Road. The conditions of approval for SUP #18081 (paraphrased) included the following:

- The subject parcels shall be combined into one parcel prior to submitting for site plan review.
- Vehicle service and repair and used car sales are permitted only ancillary to new car sales.
- Painting and/or bodywork of motor vehicles on the property is prohibited.
- Site and building lighting and fixtures shall comply with Article VII of Chapter 38 of the Code of Ordinances and shall be subject to the approval of the Director of Community Planning and Development. Exterior lighting fixtures on the north side of the buildings on the property shall include motion detectors and be turned off during non-business hours except when activated for security purposes by the motion detectors.
- Landscaping on the property shall generally comply with the landscape plan prepared by Kebs, Inc. Final landscape materials shall be provided in quantities equal to or greater than those depicted on the submitted landscape plan. The final landscape plan shall comply with all applicable sections of the Ordinance pertaining to landscaping.

The attached resolution to approve includes the aforementioned conditions, with the addition of the following condition, which was noted in the previous staff report:

- Prior to the issuance of a certificate of occupancy Powell Road shall be paved from the northern extent of the property frontage southward to Grand River Avenue in accordance with the standards of the Ingham County Road Department.

Special Use Permit #19101 (LaFontaine Chrysler Dodge Jeep & Ram of Okemos)
Planning Commission (August 26, 2019)
Page 2

Planning Commission Options

The Planning Commission may approve, approve with conditions, or deny the special use permit. A resolution to approve the special use permit with conditions is attached.

- **Motion to adopt the resolution to approve Special Use Permit #19101 with conditions.**

Attachment

1. Resolution to approve.
2. Applicant's response to the special use permit review criteria dated August 15, 2019.

G:\Community Planning & Development\Planning\SPECIAL USE PERMITS (SUP)\2019\SUP 19101 (LaFontaine)\SUP 19101.pc2.doc

RESOLUTION TO APPROVE

**Special Use Permit #19101
(LaFontaine Chrysler Dodge Jeep & Ram of Okemos)**

At a regular meeting of the Planning Commission of the Charter Township of Meridian, Ingham County, Michigan, held at the Meridian Municipal Building, in said Township on the 26th day of August, 2019, at 7:00 p.m., Local Time.

PRESENT: _____

ABSENT: _____

The following resolution was offered by _____ and supported by _____.

WHEREAS, LaFontaine Chrysler Dodge Jeep & Ram of Okemos has submitted a request to construct a new car dealership on 7.68 acres (excluding road right-of-way) at 1510 and 1560 Grand River Avenue; and

WHEREAS, the Planning Commission held a public hearing at its regular meeting on August 12, 2019 and has reviewed staff material forwarded under a cover memorandum dated August 9, 2019; and

WHEREAS, the subject site is appropriately zoned C-2 (Commercial), which allows for a new car dealership by special use permit; and

WHEREAS, the 7.68 acre property meets the minimum lot size of four acres required by the Code of Ordinances for a new car dealership; and

WHEREAS, municipal water is available to serve the subject site and sanitary sewer is available to extend to the subject site; and

WHEREAS, the proposed new car dealership is consistent with Objective A of Goal 3 of the 2017 Master Plan to enhance the viability of Township businesses by upgrading commercial areas; and

WHEREAS, the proposed project is consistent with the general standards for granting a special use permit found in Section 86-126 of the Code of Ordinances.

NOW THEREFORE, BE IT RESOLVED THE PLANNING COMMISSION OF THE CHARTER TOWNSHIP OF MERIDIAN hereby approves Special Use Permit #19101 to construct a new car dealership at 1510 and 1560 Grand River Avenue, subject to the following conditions:

1. Approval is in accordance with the site plan prepared by Kebs, Inc. dated April 17, 2018 (Revision Date July 31, 2019) and received by the Township on August 2, 2019.
2. Approval is in accordance with the floor plan and building elevations, with the exception of the signs shown on the building, dated May 23, 2019 and received by the Township on May 24, 2019.

**Resolution to Approve
SUP #19101 (LaFontaine Chrysler Dodge Jeep & Ram of Okemos)
Page 2**

3. The two subject parcels shall be combined into one parcel prior to submitting for site plan review.
4. Vehicle service and repair and used car sales are permitted only ancillary to new car sales.
5. Painting and/or bodywork of motor vehicles on the property is prohibited.
6. Prior to the issuance of a certificate of occupancy Powell Road shall be paved from the northern extent of the property frontage southward to Grand River Avenue in accordance with the standards of the Ingham County Road Department.
7. Landscaping on the property shall generally comply with the landscape plan prepared by Kebs, Inc. dated April 17, 2018 (Revision Date July 31, 2019) and received by the Township on August 2, 2019, subject to revisions as applicable. Final landscape materials shall be provided in quantities equal to or greater than those depicted on the submitted landscape plan. The final landscape plan shall comply with all applicable sections of the Ordinance pertaining to landscaping.
8. Approval of the special use permit is subject to all conditions placed on Wetland Use Permit #19-03 by the Planning Commission.
9. Site plan review approval is required to construct the proposed new car dealership. The final site plan, building elevations, and landscape plan shall be subject to the approval of the Director of Community Planning and Development.
10. The applicant shall obtain all necessary permits, licenses, and approvals from the Ingham County Road Department, Michigan Department of Transportation, Ingham County Drain Commissioner, Michigan Department of Environment, Great Lakes, and Energy, and the Township, as applicable. Copies of all permits, licenses, and approval letters shall be submitted to the Department of Community Planning and Development.
11. Utility, grading, and storm drainage plans for the site are subject to the approval of the Director of Public Works and Engineering and Ingham County Drain Commissioner and shall be completed in accordance with the Township Engineering Design and Construction Standards.
12. No grading or land clearing shall take place on the site until the site plan has been approved by the Director of Community Planning and Development and grading and soil erosion and sedimentation control (SESC) permits have been issued for the project.
13. All utility service distribution lines shall be installed underground.
14. Any future building additions or revisions to the site layout will require amendments to Special Use Permit #19101.
15. The final design of the trash and recycling facilities and related enclosures shall be subject to the approval of the Director of Community Planning and Development.
16. Site accessories such as railings, benches, exterior lighting fixtures, and bicycle racks shall be of commercial quality and complement the building design. The final design and location of such accessories shall be subject to the approval of the Director of Community Planning and Development.

**Resolution to Approve
SUP #19101 (LaFontaine Chrysler Dodge Jeep & Ram of Okemos)
Page 3**

17. Site and building lighting shall comply with Article VII of Chapter 38 of the Code of Ordinances and shall be subject to the approval of the Director of Community Planning and Development. Exterior lighting on the north side of the building on the property shall include motion detectors and be turned off during non-business hours except when activated for security purposes by the motion detectors.
18. The applicant shall construct the required seven foot wide pathway along the Grand River Avenue property frontage. The pathway shall be designed and constructed in accordance with Township Engineering Design and Construction Standards. The design and location of the pathway shall be subject to the approval of the Director of Public Works and Engineering. The property owner shall also grant to the Township a 10 foot wide easement for the pathway.
19. All mechanical, heating, ventilation, air conditioning, and similar systems shall be screened from view by an opaque structure or landscape materials (if at street level) selected to complement the building. Such screening is subject to the approval of the Director of Community Planning and Development.

ADOPTED: YEAS: _____

NAYS: _____

STATE OF MICHIGAN)

) ss

COUNTY OF INGHAM)

I, the undersigned, the duly qualified and acting Chair of the Planning Commission of the Charter Township Meridian, Ingham County, Michigan, DO HEREBY CERTIFY that the foregoing is a true and a complete copy of a resolution adopted at a regular meeting of the Planning Commission on the 26th day of August, 2019.

John Scott-Craig
Planning Commission Chair

LaFontaine

Automotive Group

August 15, 2019

LaFontaine Okemos

Charter Township of Meridian

Planning Commission

Dear Commissioners,

Please accept the following responses to the special land use standards.

1. The project is consistent with the content and purposes of this chapter because an automotive dealership is an acceptable use in C2 zoning with special use approval.
2. The project is consistent with applicable land use policies since the site does not have any special designation on the township green space plan.
3. The proposed project is harmonious and appropriate in appearance since we incorporated the townships standards in our design. Our impervious surface coverage as proposed is 49%, where 70% is allowed. We have shown compliance with the township lighting ordinance and are offering to turn off some light fixtures after hours. Of the 9.07 acres purchased our entire development is proposed on 4.69 acres which leaves 4.3 acres undisturbed.
4. The proposed project will not adversely affect neighboring uses because our operation will have low impact for peak traffic periods, we will operate with no outside speakers, we will use A/C in our service garage to promote a quiet operation, we will use directional LED light fixtures with a lighting control system and as an attached condition, we will be paving over 600' of Powell road where it abuts our property.
5. We are improving the C2 zoned property which is in a commercial corridor, therefore adding to economic value of surrounding properties.

LaFontaine

Automotive Group



6,7. Regarding public facilities, we have submitted an application for a MDOT permit. Their feedback is the basis of our deceleration lane design. We will be working with the drain commission to finalize the storm water retention design. We have sanitary and water available on Grand River.

8. Our proposed operation will not generate excessive production of traffic, noise, smoke, fumes, glare, or odors.

9. Based on the report from the township wetland consultant, the drainage area was reduced by 0.62 acre from the previously approved design. Our proposed design provides storm water detention with no wetland fill or excavation. She states that this alternative seems to be the most feasible. She also states that the project will result in limited impact to these wetland functions or values and that there are no identified cumulative impacts. Also, noteworthy, our discharge rate as designed is not greater than the predevelopment discharge rate.

Thank you,

Gary Laundroche
LaFontaine Automotive Group
Corporate Facilities Manger



To: Planning Commission

From: Peter Menser, Principal Planner
Justin Quagliata, Assistant Planner

Date: August 23, 2019

Re: Wetland Use Permit #19-03 (LaFontaine Chrysler Dodge Jeep & Ram of Okemos), request to discharge stormwater from a detention basin into a regulated wetland at 1510 and 1560 Grand River Avenue.

The Planning Commission held the public hearing for Wetland Use Permit #19-03 at its last meeting on August 12, 2019. At the meeting the Planning Commission agreed to consider a resolution to approve the request at its next meeting. If the development project associated with the wetland use permit moves forward to site plan review, confirmation of the Ingham County Drain Commissioner's approval of the drainage activity will be required. The Michigan Department of Environment, Great Lakes, and Energy has issued a permit for the stormwater discharge from the detention pond to the wetland.

Planning Commission Options

Pursuant to Section 22-157(1) of the Code of Ordinances, the Planning Commission has the option to approve, approve with conditions, or deny Wetland Use Permit #19-03. Based on the submittal date of July 23, 2019 and the 90 day review timeline established in the State Wetland Act and the Township's Wetland Ordinance, the deadline for action is October 21, 2019. A resolution to approve the wetland use permit with conditions is provided.

- **Motion to adopt the resolution to approve Wetland Use Permit #19-03 with conditions.**

Attachment

1. Resolution to approve.

RESOLUTION TO APPROVE

**Wetland Use Permit #19-03
(LaFontaine Chrysler Dodge Jeep & Ram of Okemos)**

RESOLUTION

At a regular meeting of the Planning Commission of the Charter Township of Meridian, Ingham County, Michigan, held at the Meridian Municipal Building, in said Township on the 26th day of August, 2019, at 7:00 p.m., Local Time.

PRESENT: _____

ABSENT: _____

The following resolution was offered by _____ and supported by _____.

WHEREAS, LaFontaine Chrysler Dodge Jeep & Ram of Okemos requested a wetland use permit (Wetland Use Permit #19-03) to drain stormwater from a detention pond at 1560 Grand River Avenue to a regulated wetland at 1510 Grand River Avenue; and

WHEREAS, the Township Wetland Protection Ordinance, Section 22-151, requires a wetland use permit for the proposed activity impacting a regulated wetland; and

WHEREAS, the Planning Commission held a public hearing and discussed the wetland use permit at its meeting on August 12, 2019 and has reviewed the staff material forwarded under a cover memorandum dated August 9, 2019; and

WHEREAS, the Environmental Commission and the Township wetland consultant recommended approval of the wetland use permit with conditions; and

WHEREAS, the Michigan Department of Environment, Great Lakes, and Energy issued a permit for the project on November 23, 2016.

NOW THEREFORE, BE IT RESOLVED THE PLANNING COMMISSION OF THE CHARTER TOWNSHIP OF MERIDIAN hereby approves Wetland Use Permit #19-03 to drain stormwater from a detention pond at 1560 Grand River Avenue to a regulated wetland at 1510 Grand River Avenue, subject to the following conditions:

1. Approval is based on the wetland application, drainage calculations, and related plans prepared by Kebs, Inc. dated October 12, 2016 and received by the Township on July 23, 2019.
2. Approval of the wetland use permit is contingent on the approval of Special Use Permit #19101.
3. Approval of the wetland use permit is in accordance with the Michigan Department of Environment, Great Lakes, and Energy (EGLE) approval granted on November 23, 2016.
4. The applicant shall submit a construction plan detail noting the design of the check dam/level spreader, including the volume and area of riprap and stone placement.



To: Planning Commission

From: Mark Kieselbach, Director of Community Planning and Development
Justin Quagliata, Assistant Planner

Date: August 23, 2019

Re: Special Use Permit #19-74011 (Michigan Montessori), appeal of approved special use permit to add a 0.50 acre parcel to Montessori Radmoor School property at 2745 Mt. Hope Road.

The Planning Commission held a public hearing on the appeal of Special Use Permit (SUP) #19-74011 at its August 12, 2019 meeting. At the meeting the Planning Commission agreed to consider a resolution to affirm the decision of the Director of Community Planning and Development, thereby approving the minor amendment of Special Use Permit #19-74011.

Montessori School intends to utilize the subject site as open space after demolishing the vacant house on the property. The garage on the site would be utilized for storage. Any other work on site would require an amendment to the SUP, decided by either the Director of Community Planning and Development or the Planning Commission.

Planning Commission Options

In its determination of the appeal, the Planning Commission may take, but is not limited to, any of the following actions:

1. Affirm the decision of the Director of Community Planning and Development with or without modification.
2. Reverse the decision of the Director of Community Planning and Development and state its reason therefor.
3. Refer the matter back to the Director of Community Planning and Development for further hearings or other action prior to final determination of the appeal by the Planning Commission.
4. Refer the matter to the Zoning Board of Appeals if the issue appears to be a matter of interpretation of the provisions of the zoning ordinance.

A resolution to affirm the decision of the Director is provided.

- **Motion to adopt the attached resolution to affirm the decision of the Director of Community Planning and Development, thereby approving the minor amendment of Special Use Permit #19-74011.**

Attachment

1. Resolution to affirm the decision of the Director of Community Planning and Development.

RESOLUTION TO AFFIRM

**Special Use Permit #19-74011
Michigan Montessori
2745 and 2749 Mt. Hope Road**

RESOLUTION

At the regular meeting of the Planning Commission of the Charter Township of Meridian, Ingham County, Michigan, held at the Meridian Municipal Building, in said Township on the 26th day of August, 2019, at 7:00 p.m., Local Time.

PRESENT: _____

ABSENT: _____

The following resolution was offered by _____ and supported by _____.

WHEREAS, Michigan Montessori requested a minor amendment to Special Use Permit #74011 to add 0.50 acre of land area to Montessori Radmoor School property at 2745 Mt. Hope Road; and

WHEREAS, the subject site, a 0.50 acre (21,780 square feet) parcel zoned RR (Rural Residential), is located at 2749 Mt. Hope Road; and

WHEREAS, the Director of Community Planning and Development held a public hearing on Special Use Permit #19-74011 on June 25, 2019 and approved Special Use Permit #19-74011 on June 28, 2019; and

WHEREAS, Sergey Baryshev and Evgenia Barysheva appealed the decision of the Director of Community Planning and Development to approve Special Use Permit #19-74011; and

WHEREAS, in accordance with Section 86-188(1) of the Code of Ordinances the Planning Commission held a public hearing on the appeal at its regular meeting on August 12, 2019; and

WHEREAS, the Planning Commission has reviewed staff material forwarded under a cover memorandum dated August 9, 2019 and considered testimony at the appeal hearing held on August 12, 2019; and

WHEREAS, the subject site is appropriately zoned RR (Rural Residential), where private schools are allowed by special use permit as a nonresidential use in a residential zoning district; and

WHEREAS, the requested minor amendment is consistent with the general standards for granting a special use permit found in Section 86-126 of the Code of Ordinances, such as the project is consistent with the intent and purposes of this Chapter and the project will not directly or indirectly have a substantial adverse impact on the natural resources of the Township; and

WHEREAS, the request is consistent with the additional site location standards for nonresidential structures and uses in residential districts provided in Section 86-654(e) of the Code of Ordinances, such as the school is located at the eastern edge of the RR (Rural Residential) zoning district and access to the school is from a primary road.



To: Planning Commission

From: Peter Menser, Principal Planner

Date: August 23, 2019

Re: **Tentative Preliminary Plat #19012 (Giguere Homes)**, proposed seven lot subdivision titled Sanctuary 3 located on the north side of Robins Way, east of Hulett Road.

The Planning Commission held the public hearing for Tentative Preliminary Plat #19012 at its meeting on August 12, 2019. At the meeting the Planning Commission agreed to consider a resolution to recommend approval of the tentative preliminary plat at its next meeting on August 26, 2019. The next step in the review process is consideration by the Township Board.

Planning Commission Options

The Planning Commission may recommend approval, approval with conditions, or disapproval of the tentative preliminary plat within 60 days of the plat being filed with the Township. The application was deemed complete on July 26, 2019 therefore the Planning Commission must make its recommendation no later than the September 23, 2019 regular meeting. A resolution to recommend approval of the tentative preliminary plat with conditions is provided.

- **Motion to adopt the resolution recommending approval of Tentative Preliminary Plat #19012 with conditions.**

Attachment

1. Resolution to recommend approval.

G:\Community Planning & Development\Planning\PLATS (PLAT)\2019\Plat #19012 (Giguere) Sanctuary 3\Tentative Preliminary Plat\Staff Reports\TPP 19012.pc2.docx

RESOLUTION TO APPROVE

**Tentative Preliminary Plat #19012
(Giguere Homes)**

At a regular meeting of the Planning Commission of the Charter Township of Meridian, Ingham County, Michigan, held at the Meridian Municipal Building, in said Township on the 26th day of August, 2019, at 7:00 p.m., Local Time.

PRESENT: _____

ABSENT: _____

The following resolution was offered by _____ and supported by _____.

WHEREAS, Giguere Homes has requested tentative preliminary plat approval for a single family subdivision identified as Sanctuary 3 consisting of seven lots on 7.36 acres located on the north side of Robins Way, east of Hulett Road; and

WHEREAS, the Planning Commission held the public hearing for Tentative Preliminary Plat #19012 at its meeting on August 12, 2019 and has reviewed the staff material forwarded under a cover memorandum dated August 8, 2019; and

WHEREAS, the proposed tentative preliminary plat meets the conditions established in Rezoning #18160; and

WHEREAS, the proposed tentative preliminary plat complies with the Township’s Land Division Ordinance and with the Land Division Act of the State of Michigan (MCL 560.101 et seq., as amended); and

WHEREAS, all lots in the proposed tentative preliminary plat meet or exceed the minimum lot width and minimum lot area requirements of the RAAA (Single Family-Low Density) zoning district; and

WHEREAS, the proposed tentative preliminary plat will be adequately served by public water and sewer utilities; and

NOW THEREFORE, BE IT RESOLVED THE PLANNING COMMISSION OF THE CHARTER TOWNSHIP OF MERIDIAN hereby recommends approval of Tentative Preliminary Plat #19012, subject to the following conditions:

1. Approval is recommended based on the revised Tentative Preliminary Plat prepared by Enger Surveying and Engineering dated July 12, 2019 and received by the Township on July 15, 2019.
2. Approval is recommended subject to the 11 conditions established in Rezoning #18160.
3. The applicant shall obtain all necessary permits and approvals from the Ingham County Drain Commissioner, Ingham County Road Department, Michigan Department of Environment, Great Lakes, and Energy (EGLE), Ingham County Health Department, and the Township. Copies of all permits and approval letters shall be submitted to the Department of Community Planning and Development.

Resolution to Recommend Approval

TPP #19012 (Giguere Homes)

Page 2

4. Final utility plans shall be subject to the approval of the Director of Public Works and Engineering and shall be completed in accordance with Township Engineering Design and Construction Standards.
5. Storm sewer leads shall be provided to each lot in the subdivision. The final location of the leads shall be subject to the approval of the Director of Public Works and Engineering. All residences constructed in the subdivision shall be connected to the leads.
6. An overall grading plan for the subdivision shall be required with detailed construction plans prior to issuance of any building permits.
7. No grading, tree removal, or subdivision improvements shall occur on the site until Final Preliminary Plat approval is granted by the Township Board, a pre-construction meeting has been held, and the Engineering Department has issued authorization to proceed.
8. Prior to any construction or grading on the site the applicant shall install silt fencing at the upland edge of water feature setbacks. The silt fencing shall be removed after construction once the area is stabilized and vegetation has been established.
9. The property owner shall establish and record easements for the five foot and 10 foot tree buffer areas identified on the tentative preliminary plat.
10. Trees identified for protection during construction and the means of protection shall be identified prior to approval of the final preliminary plat.
11. Natural Preservation Area Park Number 1 and Number 2 shall be identified as private parks.
12. The property owner shall submit evidence that the 20 foot deed restricted preservation area on the north side of the tentative preliminary plat has been added and recorded to the deeds for Lots 3, 4, and 5 prior to the sale of lots.
13. Five foot wide concrete sidewalks shall be constructed around the proposed Junco Drive cul-de-sac. The final location of the sidewalk shall be subject to the approval of the Director of Public Works and Engineering. The design and construction of the sidewalk shall be in accordance with Township Engineering Design and Construction Standards.
14. The natural vegetation strip associated with wetlands and open county drain shall be clearly identified with permanent markers. The size, number, location, and the language on the markers shall be subject to the approval of the Director of Community Planning and Development.
15. Interior street trees shall be required along the proposed Junco Drive cul-de-sac within the subdivision. Species and location of the trees shall be subject to the approval of the Director of Community Planning and Development and the Ingham County Road Department.
16. Any wellhead(s) located on the site shall be properly closed and abandoned per the requirements of the Ingham County Health Department and the Township, prior to the issuance of any permit for construction activity, including grading permits.

Resolution to Recommend Approval

TPP #19012 (Giguere Homes)

Page 3

17. If a development entry sign is planned for the subdivision, a detailed drawing to scale shall be provided to the Department of Community Planning and Development for review and approval prior to final plat submittal showing the location and dimensions of the sign, as well as depicting the easement location for the sign.

18. The proposed Junco Drive cul-de-sac shall be constructed to Ingham County Road Department standards.

ADOPTED: YEAS: _____

NAYS: _____

STATE OF MICHIGAN)

) ss

COUNTY OF INGHAM)

I, the undersigned, the duly qualified and acting Chair of the Planning Commission of the Charter Township Meridian, Ingham County, Michigan, DO HEREBY CERTIFY that the foregoing is a true and a complete copy of a resolution adopted at a regular meeting of the Planning Commission on the 26th day of August, 2019.

John Scott-Craig
Planning Commission Chair

Part 1. Purpose and Intent

101. Overview

The purpose of this Form-Based Code (FBC) district is to create a more walkable, pedestrian-friendly and transit-supportive mixed-use environment along the Grand River Avenue corridor that aligns with the goals and objectives of the Meridian Township Master Plan. In order to maintain or create traditional urban design and to preserve and enhance community character, this district places a primary emphasis on placemaking (physical form and character) and a secondary focus on land uses. The Avenue Form District is an optional corridor overlay; either the new FBC standards or the underlying zoning district may be selected at the time of (re)development. This optional overlay FBC provides flexibility to development through parking and dimensional requirements, height bonuses when certain amenities are included, and is overall more efficient through the development submittal process. Additionally, the Grand River Avenue Design Form District Guidelines was created as a companion document to the Form-Based Code district. These guidelines serve as a design tool for redevelopment to ensure that the standards and intent of the form district are met regarding architecture, signage, lighting, streetscaping, landscaping, and street design.

102. Form District Components

This district includes the follow sub-sections:

A. Administration

Part 2. Administration covers those aspects of the application and approval process that are unique to developing in this Form District. These standards form an optional corridor overlay district, where either these standards or the underlying zoning district may be selected at the time of (re)development. If this optional overlay is selected, all aspects of these district standards apply.

B. Regulating Plan

Part 3. Regulating Plan is the application key for the Form District. Comparable to the zoning map, this plan provides specific information on the development parameters for parcels within the districts based on street frontage. The general parameters for the Regulating Plan are between the Township western boundary edge along Grand River Avenue to the railroad to the east. However, the district boundaries may be expanded in the future by Meridian Township or by the request of a property owner into the future expansion area as shown on the Regulating Plan.

C. The Building Form Standards

Part 4. Building Form Standards establishes the development standards for parcels within the Form Districts and includes building height bonuses if certain amenities are provided.

D. Parking and Loading Standards

Part 5. Parking and Loading Standards provide goals and requirements to promote a “park once” pedestrian-friendly, multi-modal environment within these districts.

E. Building Use Standards

Part 6. Building Use Standards establishes the broad range of uses allowed in the districts.

F. Definitions

Part 7. Definitions covers certain terms that are used in these district regulations in very specific ways, often excluding some of the meanings of common usage. Defined terms are shown in SMALL CAPITAL LETTERS.

103. How to Use this Form District

I want to know what is allowed for my property:

Look at the Township Zoning Map and determine if property in question is located within a Form District.

If yes:

1. Look at the REGULATING PLAN. Note the color of the fronting STREET-SPACE—this determines the applicable BUILDING FORM STANDARD (see the key located on the REGULATING PLAN).
2. Find the applicable building form standard in *Part 4. Building Form Standards*. (Note the *General Standards* in *Part 402* that apply to all properties in the Form Districts.) The BUILDING FORM STANDARD will tell you the parameters for development on the site in terms of height, placement, elements, and use.
3. Additional regulations regarding parking requirements, building materials, and permitted building uses are found in the following sections: *Part 5. Parking and Loading Standards*; and *Part 6. Building Uses*. See also Section 86-367 of the Township's *Code of Ordinances* for information on plans for the public rights-of-way.
4. See *Part 2. Administration and Application Process* for information on the development review process.

If no:

1. ***I want to modify an existing building:*** *If one of these districts is applicable to your property, determine whether your intended changes would trigger a level of code compliance by looking at the Part 2, Table 202 Applicability Levels. If yes, follow the process delineated therein (and the indicated portions of steps 2-4, above).*
2. ***I want to establish a new use in an existing building:*** *Find the property on the REGULATING PLAN and determine the applicable BUILDING FORM STANDARD. Determine whether the use is allowed by looking at the Permitted Use Table in Part 6. Building Uses. Also, determine whether your intended changes would trigger a level of code compliance by looking at the Part 2, Table 202 Applicability Levels.*
3. ***I want to change the Regulating Plan regarding my property:*** *See Part 302, Amending a REGULATING PLAN.*
4. ***I want to subdivide my property:*** *Property can only be subdivided in accordance with the procedures of Chapter 62 of the Township's Code of Ordinances. Any such subdivision of a property within a Form District shall also meet the Form District standards.*

Part 2. Administration

201. Application of Regulations

Within the Grand River Avenue Overlay all requirements of the Meridian Township Zoning Ordinance shall apply, except as modified by this Section. When applying the regulations of the Form Districts, if regulations elsewhere within this ordinance conflict or appear to conflict with the regulations in this Section, the regulations of this Section shall apply.

202. Applicability

A. Grand River Avenue Overlay Form District

Properties located in the Grand River Overlay Form District, as designated on the Zoning Map, may elect to follow the requirements of this district as outlined in this Article. If this Article is elected, development proposals shall comply with all applicable provisions of this Article, unless otherwise modified by the Township Board. Refer to Part 3. Regulating Plans and the maps therein to determine the applicability of the FBC.

Table 202 indicates when the FBC overlay and sections of the code are applicable based on different development scenarios, including new construction, expansion of a building and/or parking area, change in use, and façade changes.

Table 202: Applicability Levels	Building Form Standard: Height	Building Form Standard: Siting	Building Form Standard: Elements	Building Form Standard: Uses	Architectural Standards: Dooryard and Private	Architectural Standards: Materials	Architectural Standards: Signs	Public Realm Standards	Parking: Location	Parking: Quantity
New Construction	X	X	X	X	O	O	O	O	X	X
Expansion of Gross Floor Area:										
0%-25% of gross floor area						O	O			
26% - 50% of gross floor area	X	X	X	X	O	O	O	O	X	
51% or more of gross floor area	X	X	X	X	O	O	O	O	X	X
Changes in Use:										
Does not require additional parking or building additions				X						
Requires additional parking or expand the parking lot by more than 10% from the approved site plan			X	X	O	O		O	X	X
Expansion of Parking Area		X	X		O	O		O	X	
Façade Changes			X		O	O	O			

Note: Improvements to nonconforming sites, structures and uses are also subject to review according to *Article V, Division 6: Nonconforming*.

X: This is a required provision

O: This is an encouraged optional provision

203. Plan Review

A. Site Plan:

1. Development requiring Site Plan Review shall follow the Site Plan Review process set forth in *Article II, Division 5: Site Plan Review* and shall meet the standards described in Part 4. Building Form Standards. For traffic impact studies, certain cases may reduce the requirements per Section 203.B.
2. Provide information available on any existing or foreseen environmental issues per *Section 86-156 Review Criteria*. Building placement requirements may be reconfigured by staff if the presence of a flood plain or wetland on the parcel prohibits development envisioned by the code.
3. In addition to the Site Plan Review process, for redevelopment projects or those seeking height bonuses where a public hearing is not required the applicant is encouraged to host a public workshop with adjacent neighbors or neighborhood to provide information on the proposed development and seek input. This should typically be held before submittal of an application, but following a preapplication meeting to obtain input from the Community Planning & Development Director.
4. Approvals are obtained from the Planning Commission, Township Board, or Community Planning & Development Director, depending upon the nature of the proposed construction or use. Where Township Board approval is required, it shall be based upon the recommendation of the Planning Commission. Where the Ordinance allows the Township to grant modifications to a specific requirement, the approval authority shall be the body with the authority to grant the associated modification or waiver, based upon the standards provided in that section. Variances may only be granted by the Zoning Board of Appeals (ZBA) per *Article II*.
5. Activities and uses that are exempt from site plan approval still require a building permit. All construction or building modification is subject to Township building permit requirements of the Building Division.

B. Traffic Impact Study:

1. The Zoning Ordinance requires a traffic impact study when a use including when there is a change in a use or expansion, is expected to generate 100 or more directional trips in a peak hour, based on the current edition of the ITE Trip Generation Manual. The typical study includes an evaluation of site access points and nearby signalized intersections including current conditions, impacts associated with the development, and the mitigation needed to address those impacts.

The Community Planning & Development Director shall make a determination if a development or redevelopment under this Form-Based Code may reduce the extent of the traffic analysis required to just the operations at the site access points in consideration of the following:

- a. The development includes a mixture of uses to reduce traffic trips (refer to the methodology for trip reduction factors in the ITE Trip Manual and Handbook).

- b. Non-motorized amenities are provided to promote travel options as an alternative to driving a vehicle such as:
 - Upgraded bike parking beyond what is required (such as covered or indoor parking, bicycle repair station, showers for commuters, etc.).
 - Transit amenities, such as shelters or seating, endorsed by CATA.
 - The employer offers a program to encourage non-automotive travel such as bus passes to employees or incentives to walk, bicycle or use transit.
- c. Existing nonconforming driveways are closed to improve safety for all types of travel.
- d. Other site amenities or programs similar to the above.

204. Deviations from Approved Site Plan

Upon application and payment of the fee in the amount established in the schedule of fees adopted by the Township Board, modifications to an approved site plan may be granted by the Director of Community Planning and Development, provided that such changes conform to the provisions of this chapter and all other Township, county, state, and federal laws and regulations.

For building height, the following modifications are permitted to be granted by the Director of Community Planning and Development. For additional height increases, please refer to 402.B Height Bonuses:

A. Height

1. Up to five percent for any cumulative increase (or decrease) in building height, as long as it does not exceed the maximum (or minimum) allowed building height in feet.
2. Finished floor elevation – up to six inches.

Part 3. Regulating Plans

301. Purpose and Intent

- A. A REGULATING PLAN is the controlling document and principal tool for implementing the Form Districts. It is a subset of the zoning map.
- B. The REGULATING PLAN makes the Form District development standards place-specific by:
1. identifying the boundaries of the district;
 2. laying out a specific street and BLOCK configuration, including new streets; and
 3. designating the BUILDING FORM STANDARDS for each STREET-SPACE (BUILDING FORM STANDARDS are regulated in *Part 4. Building Form Standards*);

302. Expanding District or Amending Regulating Plan

- A. Amendments to the adopted REGULATING PLAN are permitted as follows:
1. An expansion of the Regulating Plan into the future expansion area as identified on the Regulating Plan (303).
 2. Additional adjustments may be allowed subject to the process and procedures enumerated in *Part 2. Administration*.
 3. Any other changes to an adopted REGULATING PLAN shall meet all the criteria of this chapter and will require a rezoning.

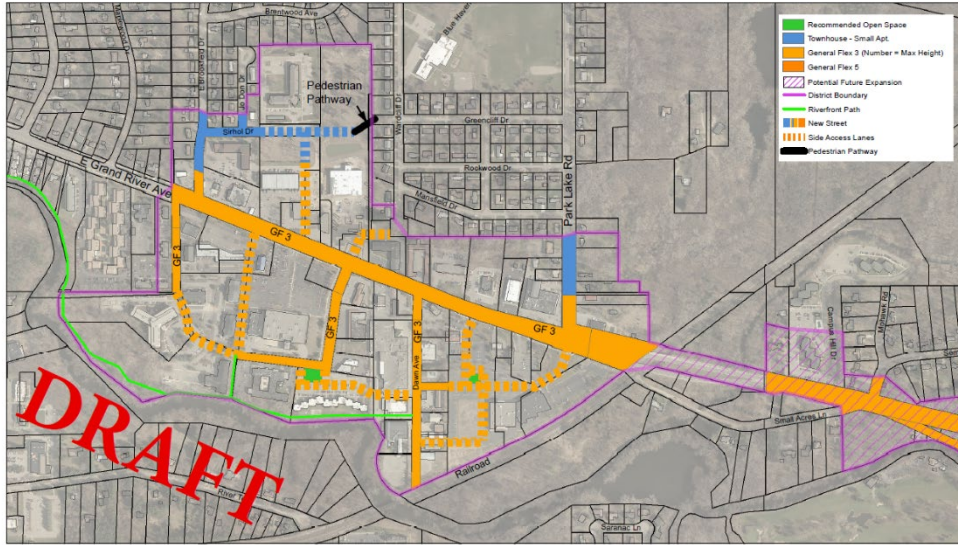
303. Regulating Plan Configurations

A. Streets

A high level of connectivity is desired in the Form District to provide people with ample opportunity to walk, bicycle or travel by vehicle within the district without a need to drive along Grand River Ave. for short trips. As sites redevelop, this connectivity shall be provided through an internal transportation network that links to the existing or planned non-motorized systems, and to existing local streets. New connections between sites should be provided as shown on the Regulating Plan including new streets, rear service drives, links between parking lots, and non-motorized pathways. The locations shown are flexible and may be aligned in a different manner provided the goal for connectivity is met. These street connections may be public streets that meet the standards of the Ingham County Roads Department or private roads that meet the requirements of Meridian Township.

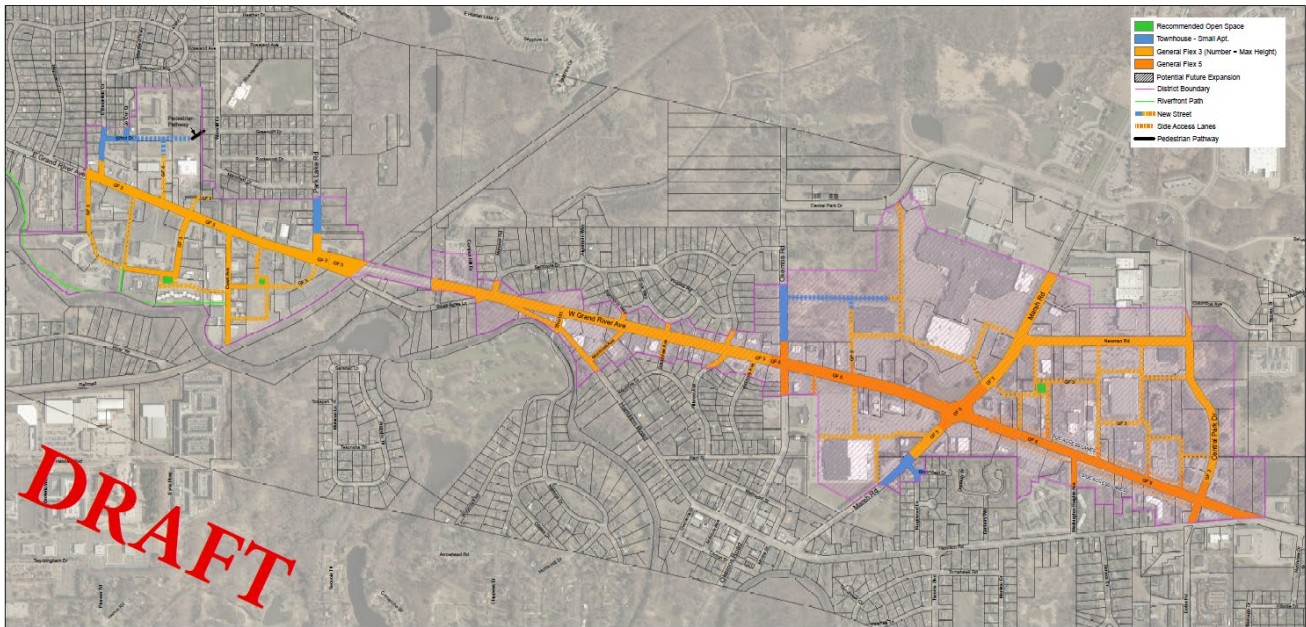
303. The Regulating Plans

The following is a small-scale reproduction of the REGULATING PLAN prepared for the Avenue Form District and a future expansion area of the REGULATING PLAN. Digital versions of this plan are available from the Township



SHAPING THE AVENUE DRAFT REGULATING PLAN FOR MERIDIAN TOWNSHIP, MICHIGAN, ON GRAND RIVER AVENUE Not to Scale
UNIQUE PLACES AND COMMUNITY SPACES JUNE 2019 DRAFT

Grand River Avenue Regulating Plan



SHAPING THE AVENUE JUNE 2019 DRAFT DRAFT REGULATING PLAN FOR MERIDIAN TOWNSHIP, MICHIGAN, ON GRAND RIVER AVENUE Not to Scale
UNIQUE PLACES AND COMMUNITY SPACES

Grand River Avenue Future Expansion Regulating Plan

Part 4. Building Form Standards

401. Purpose

A. The goal of the BUILDING FORM STANDARDS is the creation of a vital and coherent PUBLIC REALM through the creation of good STREET-SPACE. The form and function controls on building frontages work together to frame the STREET-SPACE while allowing the buildings great flexibility behind their FAÇADES.

B. The BUILDING FORM STANDARDS set the basic parameters governing building form, including the building envelope (in three dimensions) and certain required or permitted functional elements, such as FENESTRATION (windows and doors), STOOPS, BALCONIES, and FRONT PORCHES. A REGULATING PLAN identifies the applicable BUILDING FORM STANDARD(s) for all parcels within a Form District.

C. The BUILDING FORM STANDARDS establish the rules for development and redevelopment on private lots.

402. General Provisions

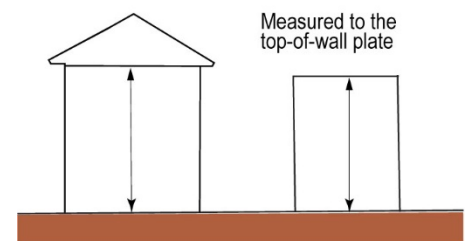
The following apply throughout the Form Districts, to all BUILDING FORM STANDARDS, unless expressly stated otherwise within an individual BUILDING FORM STANDARDS or otherwise designated on the REGULATING PLAN.

A. Height

1. The height of all buildings is measured in STORIES, with an ultimate limit in feet, measured from the average fronting sidewalk elevation to the top of the wall plate. (See figure 402.A)
2. The required minimum building height designated in each BUILDING FORM STANDARD shall be satisfied at the front building FAÇADE back to a minimum depth of 30 feet.
3. The GROUND STORY finished floor elevation requirements are measured from the average fronting sidewalk elevation at the front building FAÇADE.

B. Height Bonus

1. The Township may permit an increase in height up to four stories (or 52 feet, whichever is less) and up to five stories (or 60 feet, whichever is less) if at least the following is provided in addition to amenities prescribed in 402.B.2.
 - The site plan contains a higher level of amenities to support walking, bicycling and transit travel that is required by this overlay, emphasizing a minimum of conflict points between vehicles and pedestrians and cyclists. All of the following would need to be included: a designated pedestrian system throughout the site that connects to the Township's pedestrian/bicycle system identified in the Township's Greenspace Plan, additional bicycle amenities beyond what is required, and design features that support transit use that are endorsed by CATA.
2. An additional three amenities are required to permit an increase in height up to four stories (or 52 feet, whichever is less) and additional 4 amenities are required to permit an increase in height up to five stories (or 60 feet, whichever is less):



402.A Height measurement relative to the fronting sidewalk illustration

- Parking is provided within the footprint of the building, provided that access to this parking shall be from the side or rear, and that parking is not placed along the front building façade. Any parking within the building shall be designed to match the materials and architecture of the remainder of the building.
- Open space accessible and visible to the public shall be provided as described in the “Squares and Civic Greens” section of the Grand River Avenue Design Guidelines and public art at 1% of the project cost designed to withstand natural elements and reasonable public contact for at least 10 years to be approved by the Township and is provided in a location visible to the public.
- Streetscape elements are provided including street trees and amenities, as described in the Streetscape section of the Grand River Avenue Design Guidelines.
- Incorporate quality building materials and site design as described in the Grand River Avenue Design Guidelines, related to facades and architecture, signage, and lighting.
- Existing driveways are closed or consolidated to bring the site into greater conformity with the Access Management Standards of Meridian Township and MDOT’s standards.
- Minimize environmental impacts by using green building and site development techniques, such as an alternative energy system, green roofs, electric car charging stations, significantly increased pervious surfaces, etc.
- The site contains a complementary mix of uses, such as residential with commercial or office.
- For residential, some dwelling units are specifically designed to accommodate those with physical disabilities (consistent with “Universal Design” standards).

C. Placement

1. Front, side and rear lot setbacks, where required, are specified in the individual BUILDING FORM STANDARD.
2. No part of any building may be located forward of the minimum front setback except overhanging eaves, AWNINGS, SHOPFRONTS, BAY WINDOWS, STOOPS, steps, or BALCONIES. STOOPS and steps shall not be located within a 5’ minimum CLEAR SIDEWALK area. Handicapped ramps, approved by the Community Planning & Development Director, may be located within the DOORYARD.
3. The maximum building footprint is specified in the individual BUILDING FORM STANDARDS. Publicly accessible parking structures built according to this Code are not included in the calculation of the maximum building footprint.

D. Elements

1. Fenestration

- a. FENESTRATION is regulated as a percentage of the FAÇADE between floor levels. It is measured as glass area (including MUNTINS and similar window frame elements with a dimension less than one inch) and/or open area within the wall.

- b. Blank lengths of wall exceeding 25 linear feet are prohibited on all FAÇADES below their 3rd STORY.
- c. At least one functioning entrance shall be provided along each GROUND STORY FAÇADE.
- d. Windows shall not direct views into an adjacent private lot where the COMMON LOT LINE is within 20 feet.

2. Façade Projections

- a. GROUND STORY AWNINGS shall have a minimum 9-foot CLEAR HEIGHT above the sidewalk and a minimum five-foot depth, measured from the FAÇADE. The maximum depth is to back-of-curb or the TREE LAWN edge, whichever is less.
- b. BALCONIES:
 - i. Shall not be located within 2 feet of any COMMON LOT LINE and shall not encroach into the public right-of-way.
 - ii. BALCONIES may be a single level or multiple BALCONIES stacked vertically for multiple STORIES.
- c. BAY WINDOWS shall have an interior clear width of between four and eight feet at the main wall and shall project no more than 42 inches into the setback.
- d. STOOPS and FRONT PORCHES:
 - i. Shall not encroach into the CLEAR SIDEWALK.
 - ii. FRONT PORCHES may be screened (insect screening) when all architectural elements (columns, posts, railings, etc.) occur on the outside of the screen facing the STREET-SPACE
 - iii. Finished floor height shall be no more than 8 inches below the first interior finished floor level of the building.

E. On-Site Vehicle Parking and Location

- 1. Vehicle parking shall be located, at minimum, behind a front parking setback of 8 feet behind the front building FAÇADE.
- 2. Parking may occur forward of the parking setback if:
 - a. Interior: It is completely within the building envelope; the floor level is at least five feet below grade; and FENESTRATION is not greater than 20%.
 - b. Exterior: The required parking setback may be reduced to 5 feet behind the front building FAÇADE if a minimum 3 ft. tall masonry GARDEN WALL is provided.
- 3. Any portion of a parking structure within 30 feet of a building constructed under this code shall not exceed that building's primary ridge or parapet height.
- 4. Refer to Part 5. Parking and Loading Standards for additional regulations, including parking minimum requirements and special parking standards.

F. Civic Buildings

CIVIC BUILDINGS are exempt from the BUILDING FORM STANDARD provisions except those that relate to single-family detached dwelling districts.

G. Pedestrian, Bike, and Transit-Friendly Design

- 1. Emphasis shall be placed on enhancing the overall walkability and safety of the area through appropriate pedestrian, bike, and transit accommodations and streetscape improvements. Refer to the Grand River Avenue Design Guidelines

for guidance on the design of the streetscape, landscaping, transit amenities, streets, and open space areas.

2. A pedestrian crosswalk and sidewalk of a minimum of 5 feet wide (min 7 feet when adjacent to parking to accommodate vehicle overhang) shall be provided in parking lots that provides a direct connection from the street side non-motorized path or sidewalk to the entrance of the building.
3. The crosswalk or path within a parking lot shall be clearly delineated with striping or use of other non-slip materials that contrast with parking lot's primary material.
4. Appropriate yield, crosswalk markings or traffic calming design elements shall be provided to indicate where pedestrians are crossing vehicular aisles in a parking lot.
5. Bus stops and/or connections and amenities that support use of transit, are encouraged with the endorsement of CATA. Any bus stops shall be located at a place that provides a convenient pedestrian crossing of Grand River Ave. (refer to Pedestrian Crossing Guidelines in the Design Guidelines).
6. All sites shall meet the Bicycle Parking standards per Section 86-760 of the Zoning Ordinance.

H. Access Management

1. As redevelopment occurs, existing driveways that do not meet current standards should be removed or redesigned, to the extent practical, as determined by the Township. These changes should be a site improvement priority to improve safety for all types of travel, traffic flow, and the overall appearance of the district. In particular, elimination of access points are a priority where close to signalized intersections or where there is a poor offset spacing from driveways on the opposite side of Grand River Ave. Shared access may be required with adjacent sites where the driveway spacing standards cannot be met. Additionally, if there is there is a rear access drive located on an adjacent lot, than the development should provide a connection to that rear drive. Easements shall be provided for shared access with adjacent sites or cross circulation between adjacent parking lots.
2. Access points for new driveways shall meet the Township's standards described in Section 86-441 Grand River Avenue Corridor Access Management Overlay District along with the standards of the MDOT (for Grand River Ave.) or Ingham County Roads Department (for all other streets).

403. General Flex Avenue

ILLUSTRATIONS AND INTENT

Note: These photos and statements are provided as illustrations of intent and are advisory only. They do not have the power of law. Refer to the standards on the following pages for the specific prescriptions and restrictions of the BUILDING FORM STANDARD.

This is the basic urban STREET-SPACE FRONTAGE. It fully defines the street edge and accommodates a range of uses, including residential, office, institutional, and retail. This frontage is in the most intense areas, generally along the Avenue. It is anticipated that there will be significant pedestrian traffic along this frontage.

The maximum height varies by location and is designated on the REGULATING PLAN.

*Retail buildings fronting onto the street
New development with parking behind the building*



Retail buildings with DOORYARD



Residential buildings with raised DOORYARDS



Buildings with DOORYARD, sidewalk, TREE LAWN



A. General Flex Avenue: Building Form Standards

(1) Height

Minimum (GF 3)	1 story, 16 ft.
Maximum (GF 3)	3 stories, 45 ft.
Maximum (Height Bonus or Future GF 5)	5 stories, 60 ft
Ground Floor Elevation - Residential Units (min.)	3 ft.
Second Floor Finished Elevation (min.)	18 ft.

(2) Placement

Front Setback (minimum)	Grand River Ave: 15 ft. from ROW ¹ All other streets: 5 ft. from ROW
Front Setback (maximum)	Grand River Ave: 25 ft from ROW ¹ All other streets: 15 ft from ROW

¹Buildings shall be placed between 72' to 75' from ROW where Side Access Lanes are required on the Regulating Plan. Placement may be adjusted in coordination with the Community Planning & Development Director at time of development review; access lanes shall align on adjacent parcels.

Front yard parking Not permitted (see 402.E)

Parking front yard screening A 8' min. landscape buffer shall be provided; may be reduced to 5 ft. with a min. 3 ft. masonry GARDEN WALL

Parking rear yard screening A 20' minimum landscape buffer if adjacent to residential or 15' minimum landscape buffer if adjacent to non-residential

Side Setbacks (min.) 0 ft.

Rear Setback with SHARED DRIVE (min.) 10 ft.

Rear setback with no SHARED DRIVE (min.) 25 ft.

Adjacent single-family setback (side and rear) Setback equal to the rear setback of adjacent district

Adjacent single-family screening (side and rear) 6 ft. opaque screenwall or fencing within 1 ft. of common lot line

Building footprint (max.) 15,000 sq. ft.

(3) Architectural Elements

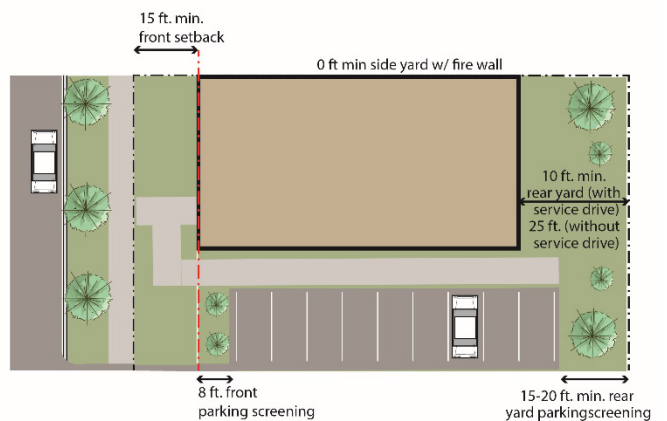
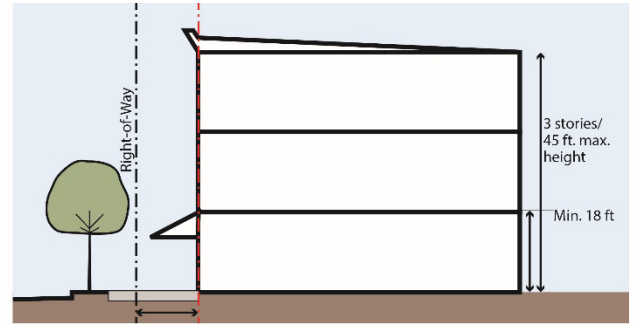
GROUND STORY FENESTRATION 40 to 90%

Upper Story FENESTRATION 25 to 80%

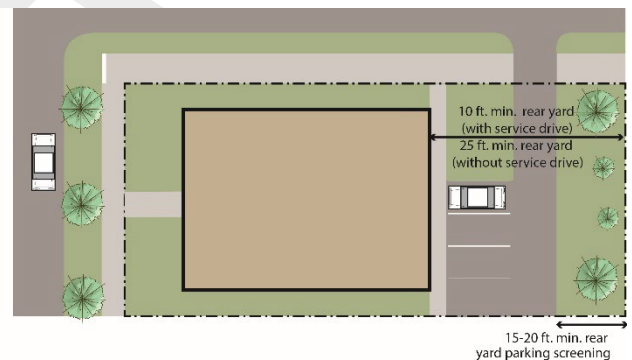
Buildings greater than 3 stories shall be designed to reduce apparent mass by including a clearly identifiable base, body, and top, with horizontal elements defining these components.

Blank walls exceeding 25 linear feet are prohibited.

Elevations facing a street shall contain a minimum of 75% masonry such as brick or stone



Side Yard Parking Option



Rear Yard Parking Option (Preferred)

405. Storefront

ILLUSTRATIONS AND INTENT

These photos and statements are provided as illustrations of intent and are advisory only. They do not have the power of law. Refer to the standards below and on the previous pages for the specific prescriptions and restrictions of this BUILDING FORM STANDARD. Where photos or statements may be inconsistent with the regulations, the regulations prevail.

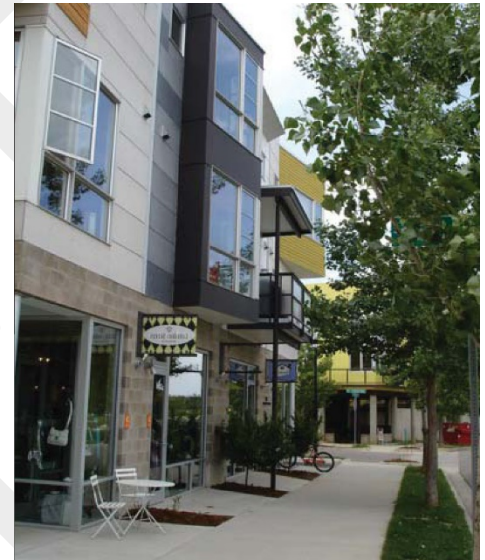
Where Storefront Frontage is designated on the REGULATING PLAN, the General Flex BUILDING FORM STANDARDS (previous pages) apply, except that the GROUND STORY configuration shall be that of a SHOPFRONT with uses limited to COMMERCE (RETAIL included).

- A. Single panes of glass shall not be permitted larger than 11 feet in height by 6 feet in width.
- B. GROUND STORY windows may not be made opaque by window treatments (excepting operable sunscreen devices within the conditioned space). A minimum of 80% of the window surface shall allow a view into the building interior for a depth of at least 15 feet.

ELEMENTS	
SHOPFRONT FENESTRATION	60 to 90%
FAÇADE: Max door to door ¹	60'
AWNING CLEAR HEIGHT Min	10'

Notes:

- 1. On the FAÇADE of an individual building. Applicants with phased projects must satisfy this rule for each phase of their project.



406. Townhouse/Small Apartment

ILLUSTRATIONS AND INTENT

Note: These photos and statements are provided as illustrations of intent and are advisory only. They do not have the power of law. Refer to the standards on the following pages for the specific prescriptions and restrictions of the BUILDING FORM STANDARD.

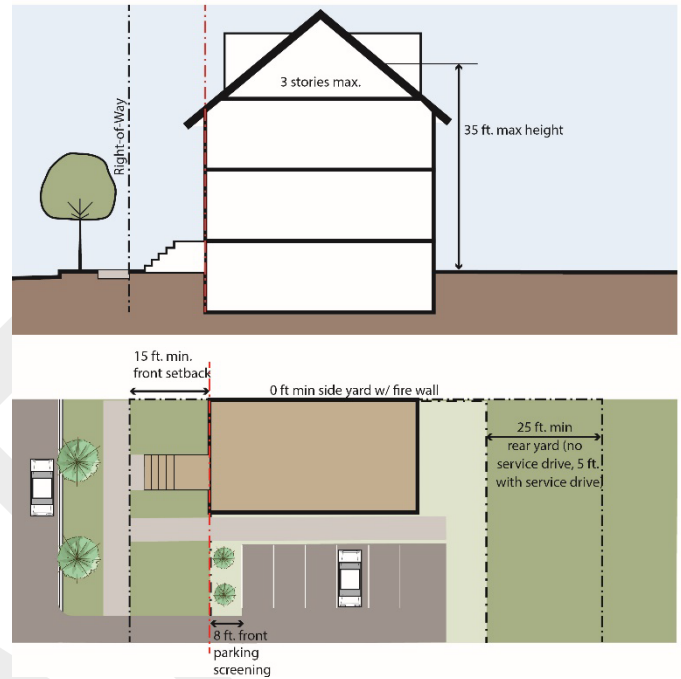
The Townhouse/Small Apartment frontage is of moderate intensity, created by a series of smaller structures—configured as single-family residential or stacked flats. This BUILDING FORM STANDARD has regular STREET-SPACE entrances, as frequently as 18 feet. The character and intensity of this frontage varies depending on the STREET-SPACE and the location of the front building line—the buildings may be placed up to the minimum setback line with STOOPS, or further back with gardens and/or FRONT PORCHES.

Similar in scale to the townhouse and rowhouse, a small apartment is of limited size and can also be used to transition from the more intense form of the Avenue to adjacent single-family neighborhoods.



A. Townhouse/Small Apartment: Building Form Standards

(1) Height	
Minimum	2 stories, 25 ft.
Maximum	3 stories, 35 ft.
(2) Placement	
Front Setback (min.)	5 ft. from ROW
Front Setback (max.)	15 ft from ROW
Front yard parking	Not permitted (see 402.E)
Parking front yard screening	A 8' min. landscape buffer shall be provided; may be reduced to 5 ft. with a min. 3 ft. masonry GARDEN WALL
STOOP Placement	A STOOP may occur forward of the Front Setback; a 5' min CLEAR SIDEWALK area shall be maintained
Side Setbacks (min.)	0 ft.
Rear Setback with SHARED DRIVE (min.)	5 ft.
Rear setback with no SHARED DRIVE (min.)	25 ft.
Adjacent single-family rear setback	Setback equal to rear setback of adjacent residential district
(3) Architectural Elements	
All Stories FENESTRATION	25 to 70%
Blank walls exceeding 25 linear feet are prohibited	
Elevations facing a street shall contain a minimum of 75% masonry such as brick or stone	



Part 5. Parking and Loading Standards

501. Intent

These Form District standards are intended to:

- A. Promote a “park once” environment with walkable nodes that will enable people to conveniently park and access a variety of commercial, residential, and civic enterprises in pedestrian-friendly environments by encouraging SHARED PARKING.
- B. Reduce fragmented, uncoordinated, inefficient, reserved single-purpose parking.
- C. Provide flexibility for redevelopment of sites.
- D. Increase visibility and accessibility of publicly available parking.

502. Minimum Parking Requirements

Parking requirements in the Grand River Avenue Overlay Form District shall be regulated by *Sec. 86-755* but with the following specific departures from that section. These parking reductions are based on an anticipated shift from single-occupant vehicle travel to walking, bicycling, transit, and car share services often associated with the mixture of uses within compact walkable areas consistent with the code.

- A. If SHARED PARKING is provided as described in *86-753*, the combined amount of parking required is reduced by up to 20% as determined by the Township based on a demonstration by the applicant that the combined peak hour utilization of the uses will not exceed 85-90% of the parking supply on a typical day.
 1. This percentage may be increased by up to 40% if the applicant provides information to demonstrate a maximum 85% of parking available is expected to be occupied during peak periods and the development features the elements described in B below.
 2. The Township may require a parking study by a qualified professional using accepted sources and methodology.
 3. In addition, the Township may require some additional parking area be “banked” for future use if the anticipated SHARED PARKING is inadequate or if a use change to one that requires significant additional parking is made.
- B. The Township may reduce the required parking further, as noted above, if the applicant demonstrates that on-site parking demand will be reduced through amenities and programs that will shift travel from single occupant vehicles to other modes of travel such as:
 - Provision of a bus stop with amenities such as a shelter, shade trees and other design features endorsed by CATA.
 - Incentives for employees to use transit, such as free transit passes or other programs endorsed by CATA.
 - Site design elements that promote walking and bicycling, such as bike racks by building entrances, indoor parking and other facilities for those who travel by bicycles.
- C. Parking Requirements by Use:
Residential:

1. 1.25 parking spaces shall be provided per multiple family unit (Grand River Avenue) with an additional 0.25 space per unit available for visitor and public use.
2. Required parking per unit may be reduced if the development provides a “car-share” system for use by residents.

Commercial centers and general retail:

1. 3.5 spaces per 1,000 sq. ft. (minimum) and 4.0 spaces (maximum¹) for buildings with a gross floor area less than 25,000 sq. ft.
2. 4.0 spaces per 1,000 sq. ft. (minimum) or 5.0 spaces (maximum¹) for buildings with a gross floor area over 25,000 sq. ft.

Restaurants, taverns & bars, nightclubs, distilleries and brew pubs, Grand River Avenue:

1. 1 space per 100 sq. ft. of usable floor area.

For uses not specifically listed above, the minimum parking requirements in the Township Zoning Ordinance shall apply.

503. Achieving Parking Requirements

- A. Parking requirements may be met either on-site or within an 800-foot walking distance of the development. The required parking must all be on the same side of Grand River Avenue however parking on the opposite side may be included if within 660' (1/8 mile) of a designated pedestrian crosswalk
- B. Parking shall be located in compliance with the parking standards in *Sections 504*, below.
- C. Bicycle Parking shall be provided as required by *Sec. 86-760*.
- D. All other parking standards of *Article XIII Off-Street Parking & Loading* shall apply.

504. Special Parking Standards

A. Joint Parking

Sites abutting one another shall physically connect their surface parking areas at the lot line to create connecting drive aisles. Where such surface parking areas lie within 50 feet of one another, a mutual access easement acceptable to the Community Planning & Development Director shall be executed. Parking lot configurations existing (*insert effective adoption date*) are exempt from this requirement.

B. On-Street Parking

If on-street parking is provided along building frontage, public street frontage, or approved alongside access lanes, those spaces may be counted towards parking requirements for the specific use.

C. Off-Street Parking

Off-street parking shall be located in compliance with the parking setback regulations for the site on which it is located, as indicated in the BUILDING FORM STANDARDS.

¹ *Maximum standards only apply to surface parking lots, not structured or underground parking.*

D. Off-Site Parking

1. Off-site parking must be located within a walking distance of 800 feet from the site it is serving.
2. The off-site parking shall be located within the Avenue Form District.
3. The off-site parking must be the subject of a long-term lease approved as to form by the Township attorney, or permanently dedicated for off-site parking use.

505. Loading

Development under this code prohibits any street-side loading facilities. Consistent rear-access and circulation on SHARED DRIVES is recommended.

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Part 6. Building Uses

601. General Provisions

A. Permitted Uses

Uses are grouped into broad categories. Permitted uses by BUILDING FORM STANDARD are shown in *Section 602*. The categories in the use table are listed in *Section 603*.

B. Use Determination

1. The Community Planning & Development Director is responsible for categorizing all uses. If a proposed use is not listed in a use category but can be said to be reasonably similar in impact on a Form District to a listed use, the Director shall treat the proposed use as a use under that category. If a proposed use is not listed in a use category, and is fundamentally different from any other listed use, the use shall be prohibited.
2. Special Use Permit: If the site has an approved Special Use Permit, than no additional Special Use Permit is required for an additional use or building on the site, unless a new use is classified as a Special Use in Table 602 or if the existing use is proposed to change to another special use (refer to Article VI Special Use Requirements and Restrictions).
3. Uses not specifically listed: When determining whether a proposed use is similar to a permitted use, the Director shall consider the following criteria:
 - a. The actual or projected characteristics of the proposed activity in relationship to the stated characteristics of each use.
 - b. Types of vehicles used and their parking and or loading requirements.
 - c. The likely impact on surrounding properties.
 - d. The intent of the Form Districts.

602. Use Table

The use table identifies the uses allowed in the respective BUILDING FORM STANDARD frontages.

		General Flex	Townhouse / Small Apt	
RESIDENTIAL	Household Living (including one, two, and three-family dwellings and multi-family dwellings)	■	■	
	Supported and Assisted Living (Group Homes and Senior Housing)	■	■	Section 86-654 of the Township's <i>Code of Ordinances</i>
COMMERCE	Office	■		
	Hotel	■	□	
	Recreation/ Entertainment	■		
	Day Care (adult and child)	■	■	children: Section 86-403 (d)(1) adults: Section 86-403 (d)(2) of Township's <i>Code of Ordinances</i>
	Retail Sales & Personal Services ¹	■		
	Restaurant/Bar/Microbreweries ¹	■		
	Vehicle Service/Gas Station ²	□		
	RESEARCH and Laboratory Facilities	□		Sec. 86-434
	Office	■		
CIVIC	Police, Fire, Municipal	■	■	

■ Permitted □ Special Use Permit Blank Box: Prohibited

Footnotes to 602. Use Table:

- Outdoor eating areas for restaurants shall be allowed in General Flex and/or Storefront frontages, subject to the Township's "outdoor displays and cafes" ordinance (see Section 86-403 (c)(6) of the Township's Code of Ordinances). A RETAIL SALES use or restaurant is permitted in the second STORY of a Storefront or General Flex site provided it is an extension of that GROUND STORY use, with equal or less floor area.
- Gas stations shall be subject to the standards in the Township's Code of Ordinances per Section 86-403 (e)(5).

Part 7. Definitions

701. Defined Terms

The following terms are defined for the purpose of the form districts. Terms not defined here may be defined elsewhere in the zoning ordinance. In such case, the definition contained in the zoning ordinance shall be used. Certain terms in these districts are used in very specific ways, often excluding some of the meanings of common usage. Where there is an apparent conflict or contradiction, the definition herein shall prevail.

AWNING. A cantilevered, projected or suspended cover over the sidewalk portion of the STREET-SPACE, or a roof like covering, usually of canvas, metal, or similar material and often adjustable, placed over the sidewalk, windows, or doors to provide protection from sun and rain. It is distinguished from a canopy because it is not permanent, nor a structural portion or architectural feature of the building and does not support substantial weight.

BALCONY. An exterior platform attached to the upper floors of the building FAÇADE.

BAY WINDOW. Generally, a U-shaped enclosure extending the interior space of the building outward of the FAÇADE (along its STREET-SPACE side).

BLOCK. An increment of land comprised of lots, COMMON DRIVES, and tracts circumscribed and not traversed by streets (PEDESTRIAN PATHWAYS excepted). BLOCKS are measured at the REQUIRED BUILDING LINE (RBL).

BUILDING FORM STANDARDS (BFS). The part of these district standards that establish basic parameters regulating building form, including: the envelope (in three dimensions); placement on the lot; and certain permitted and required building elements, such as storefronts and BALCONIES.

CLEAR HEIGHT. Within a structure, the habitable distance between the floor and ceiling. For entrances and other external building features, the unobstructed distance from the ground/sidewalk to the lowest element above.

CLEAR SIDEWALK. An area within a STREET-SPACE, the portion of the sidewalk that must remain clear of obstructions and allow public passage. In the Avenue Form District area, the CLEAR SIDEWALK width shall be 5' minimum.

COMMERCE. See USE, COMMERCE.

COMMON LOT LINES. Lot lines shared by adjacent private lots.

CONSERVATION LANDS. Areas that are not developable due to environmental constraints or easements, such as floodplains, wetlands, steep topography, wildlife preserves, etc.

DOORYARD. The area within the STREET-SPACE, extending across the entire width of the lot, between the FAÇADE of the building and the CLEAR SIDEWALK portion of the sidewalk, which may be paved or planted, depending on the *Street Type Specification* designation.

FAÇADE (Building Face). The building elevation facing the STREET-SPACE. Building walls facing private interior courts, COMMON LOT LINES, and SHARED DRIVES are not FAÇADES (they are elevations).

FENESTRATION. Openings in a wall, including windows and doors, allowing light and views between the BLOCK and/or building interior (private realm) and sidewalk and/or street exterior (PUBLIC REALM).

FRONT PORCH. The ground floor platform attached to the FAÇADE side of the main building.

GARDEN WALL. A masonry wall defining a property line or delineating a private area. (For height and gate specifications, see the BUILDING FORM STANDARDS.)

GROUND STORY. The first habitable level of a building at or above grade. The next STORY above the GROUND STORY is the second floor or STORY.

MUNTIN. A strip of wood or metal separating and holding panes of glass in a window, less than 1" in thickness. MUNTINS divide a single window sash or casement into a grid system of small panes of glass.

PRIVACY FENCE. An opaque fence along COMMON DRIVES, pedestrian pathways, and COMMON LOT LINES. See the *Building Form Standards* for height and placement specifications and *Architecture* for material and configuration standards.

PUBLIC REALM (STREET-SPACE). All space between fronting building FACADES, including streets, squares, plazas, parks, pedestrian pathways, sidewalks, parks)—including transit service operator passenger platform—but not within GARAGE ENTRIES or COMMON DRIVES.

REGULATING PLAN. The implementing plan for development within the form districts. REGULATING PLANS designate the BUILDING FORM STANDARDS. The REGULATING PLAN also shows how each site relates to adjacent STREET-SPACES, the overall district, and the surrounding neighborhoods.

SHARED DRIVE. The public right-of-way or easement for vehicles and pedestrians within a block that provides service access to the rear or side of properties, vehicle parking (e.g., garages), loading docks, utility meters, recycling containers, and garbage bins.

SHARED PARKING. Automobile parking that is visible and accessible to the public for a minimum portion of each day.

STOOP. An entry platform on the FAÇADE of a building. (See the BUILDING FORM STANDARDS for specifications.)

STORY. That space within a building and above grade that is situated between one floor level and the floor level next above, or if there is no floor above, the ceiling or roof above.

STREET-SPACE (PUBLIC REALM). All space between fronting building FACADES, including streets, squares, plazas, parks, pedestrian pathways, sidewalks, parks)—including transit service operator passenger platform—but not within GARAGE ENTRIES or COMMON DRIVES.

TREE LAWN. A continuous strip of soil area—typically covered with grass, other vegetation, bridging pavement, or sometimes porous pavers—located between the back of curb and the CLEAR SIDEWALK and used for planting street trees and configured to foster healthy street tree root systems.

USE, COMMERCE. For the purpose of these form districts, COMMERCE USES shall be considered to encompass all of the by-right and conditional uses permitted in the following Meridian zoning districts: Commercial (C1-3), Professional and Office (PO), and Research Park and Office Park (RP), unless expressly prohibited herein, and all of the CIVIC USES defined above, except transit centers.

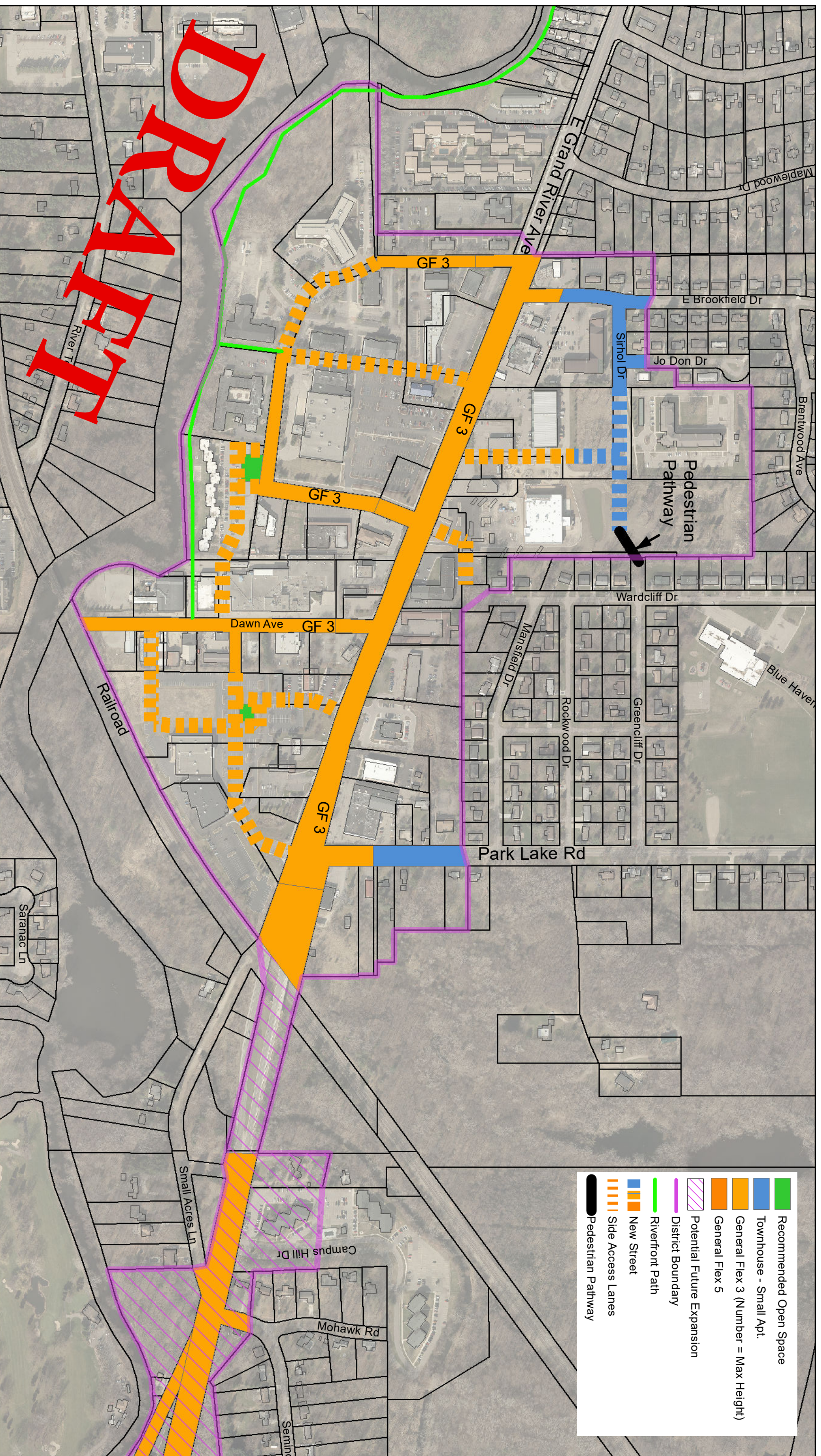
USE, RESIDENTIAL, RESIDENTIAL USES shall be considered to encompass all of the uses allowed by-right and with a conditional use permit in the residential zoning districts as defined in the Meridian zoning ordinance.

USE, RETAIL. Includes the following:

RETAIL SALES. Establishments wherein the primary use is the sale of merchandise for use or consumption by the immediate purchaser.

RETAIL SERVICE. Establishments providing services, as opposed to products, to the general public, including restaurants, hotels and motels, finance, real estate and insurance, travel agencies, health and educational services, and galleries; as well as personal services as defined in the Meridian zoning ordinance

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- Recommended Open Space
- Townhouse - Small Apt.
- General Flex 3 (Number = Max Height)
- General Flex 5
- Potential Future Expansion
- District Boundary
- Riverfront Path
- New Street
- Side Access Lanes
- Pedestrian Pathway

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Grand River Avenue Form District Design Guidelines

DRAFT June 21, 2019

Introduction

Part A. Architectural Guidelines

Part B. Signage Guidelines

Part C. Lighting, Mechanical and Service Area Guidelines

Part D. Streetscaping and Landscaping Guidelines

Part F. Street Design Guidelines

Part G. Definitions

Introduction

Shaping the Avenue is a multi-jurisdictional partnership between the municipalities of Lansing, Lansing Township, East Lansing, Meridian Township and the Capital Area Transportation Authority (CATA). The initiative realizes years of community visioning for the Michigan and Grand River avenues (the Avenue) by putting the land use and street design regulations in place to support economic development, build upon community character, and improve comfort and safety for all modes of travel – cyclists, drivers, pedestrians and transit users.

The Shaping the Avenue partners have drafted regulations that incorporate transit-oriented development (TOD) principles into form-based codes (FBC), a style of zoning ordinance that offers more direction on how buildings and streets should look (their form), as opposed to conventional zoning which is primarily focused on the types of uses allowed on land. These ordinances will guide how buildings and streets develop over time.

In Meridian Township, the Grand River Avenue Form District is intended to create a more walkable, pedestrian-friendly and transit-supportive mixed-use environment along the Grand River Avenue corridor, aligning with the goals and objectives of the Meridian Township Master Plan. The Avenue Form District is an optional corridor overlay; either the new FBC standards or the underlying zoning district may be selected at the time of (re)development. Additionally, Grand River Avenue Design Guidelines was created as a companion document to the Form-Based Code district. These guidelines serve as a design tool for redevelopment to ensure that the standards and intent of the form district are met regarding architecture, signage, lighting, streetscaping, landscaping, and street design.

Part A. Architectural Guidelines

1. Purpose and Intent

- A. These Architectural Guidelines establish basic parameters regarding functional building element configurations and a material palette for exterior building materials.
- B. These architectural guidelines serve to establish a coherent character and encourage a high caliber, lasting quality of development.
- C. In order to establish and maintain a sense of place, these standards specify an architectural aesthetic of load-bearing walls and regional materials. The guidelines also specify details, such as window proportions, roof or CORNICE configurations, SHOPFRONTS, and overhangs. Buildings should reflect and complement the traditional materials and techniques of the Mid-Michigan region.

2. General Principles

- A. Where CLEARLY VISIBLE FROM THE STREET-SPACE.
 - 1. Many of these guidelines are encouraged only where CLEARLY VISIBLE FROM THE STREET-SPACE.
 - 2. These controls concentrate on the PUBLIC REALM/views from the PUBLIC REALM and minimize interference in the private realm. For example, an architectural element that is visible only through an opening in a STREET WALL is not CLEARLY VISIBLE FROM THE STREET-SPACE.
- B. All building materials to be used should express their fundamental properties. For example, stronger and heavier materials (masonry) should be located below lighter materials (wood).

3. Building Walls

A. Purpose and Intent

FAÇADES define the PUBLIC REALM—the STREET-SPACE. All walls should express the construction techniques and structural constraints of their building materials. Simple configurations and solid craftsmanship are favored over complexity and ostentation in building form and the articulation of details.

B. Applicability

The standards in this section are encouraged for all building FAÇADE that are CLEARLY VISIBLE FROM THE STREET-SPACE.

C. Illustrations

Photographs are provided as illustrations of intent. The illustrations and statement on this page are advisory only.



Left to right: Buildings with brick FAÇADE; Cast iron SHOPFRONT with brick second STORY; Material change in a logical location



Left to right: Brick building walls; Newly constructed brick townhouses with architectural detailing; Building FAÇADES with limestone ground floor and wood siding above.

D. Primary Façade Materials.

Any of the following building materials are encouraged to be used on a minimum of 75% of the FAÇADE. This should be calculated as a percentage of the wall portion of the FAÇADE, exclusive of FENESTRATION.

1. Brick
2. Wood (or approved fiber cement siding);
3. Natural Stone (or integrally-colored synthetic, EQUIVALENT OR BETTER);
4. Stucco (cement plaster);
5. Cast iron, copper, stainless steel (18-8 or better), and titanium metal siding.

E. Secondary Materials

Any of the following materials are suggested on a maximum of 25% of the FAÇADE and additionally on all side and rear elevations.

1. All permitted primary materials;
2. Metal;
3. Ground- or Split-faced block (integrally colored);
4. Glass block;
5. Decorative tile;
6. Pre-cast masonry;
7. Synthetic materials (only above the second STORY) as approved by the Community Planning & Development Director.

F. Configurations and Techniques

1. Walls

- a. Wall openings (FENESTRATION): the horizontal dimension of the opening should not exceed the vertical dimension.
- b. Wall openings (FENESTRATION) should correspond to the interior space and should not span across building structure such as floor or wall structural thicknesses.
- c. Material changes should be made with appropriate construction details for each abutting material—as where an addition (of a different material) is built onto the original building.

2. Wood Siding and Wood Simulation Materials

- a. Horizontal siding should be configured with a maximum board exposure of 8”.
- b. Board and batten siding should have a maximum board width of 12”.
- c. Siding, shingles and shakes may be smooth or rough-sawn finish.

3. Brick, Block and Stone

- a. All masonry should be in an apparent load-bearing configuration.

4. Stucco (cementitious finish)

- a. Finish coat should be smooth or sand only, no rough textured finish.
- b. Stucco should not come in contact with the ground surface.

4. Roofs and Parapets

A. Purpose and Intent

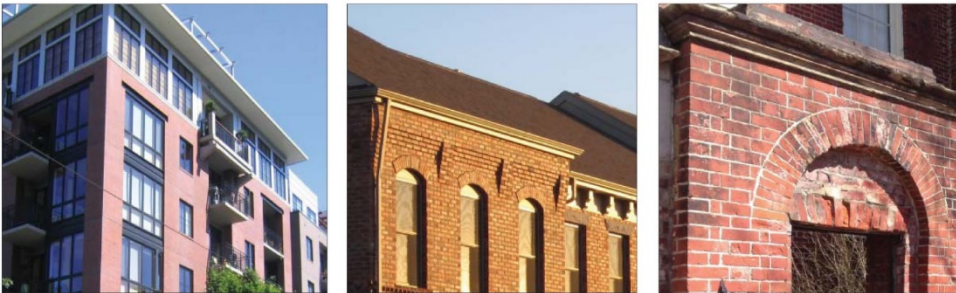
Roofs and parapets are part of the FAÇADE COMPOSITION (its crown or hat) and important to the spatial definition of the STREET-SPACE. Roofs and parapets should demonstrate common-sense recognition of the climate by utilizing appropriate pitch, drainage, and materials in order to provide visual coherence to the district. Roof forms are not interchangeable. The roof type is integral to the design of the building and its architectural character and these elements should be appropriate for the building and its FAÇADE.

B. Applicability

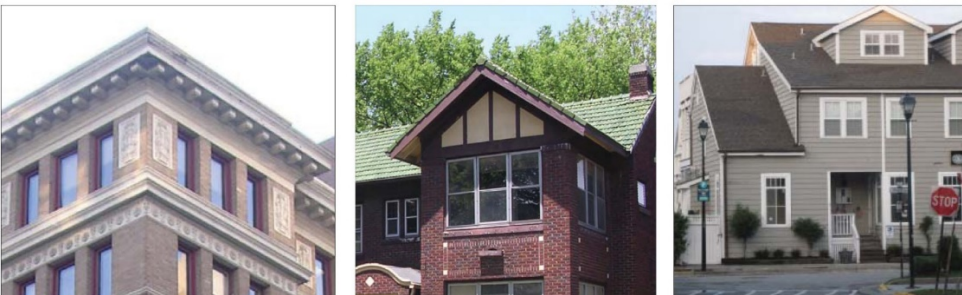
The standards in this section are encouraged for any roof or parapet that is CLEARLY VISIBLE FROM THE STREET-SPACE.

C. Illustrations

Photographs are provided as illustrations of intent. The illustrations and statement on this page are advisory only.



Left to right: Projecting CORNICE; Pitched roof with projecting CORNICE; Parapet wall with COPING



Left to right: Parapet wall with projecting CORNICE; Overhanging eave; Pitched roof

D. Materials

1. The following roofing materials are promoted:
 - a. Tile;
 - b. Slate (and equivalent synthetic or better);
 - c. Metal (standing seam, EQUIVALENT OR BETTER);
 - d. Dimensional architectural grade composition shingles; or
 - e. Wood shingles.
2. Additional roof elements include:
 - a. Skylights and solar panels;
 - b. CORNICES and soffits may be comprised of wood, vinyl, synthetic materials and/or metal, as approved by the Community Planning & Development Director; and
 - c. Gutters and downspouts may be vinyl, and/or metal, in accordance with industry standards.
3. Parapet wall materials should match the building wall.

E. Configurations and Techniques

1. Flat Roofs with Parapets

Where the roof material is not visible from an adjacent STREET-SPACE, Flat roofs with parapets are encouraged in General Flex, and Storefront frontage sites.

2. Pitched Roofs

Pitched roofs, excluding areas behind parapet walls should be pitched..

3. Overhang Requirements

- a. Eaves should overhang 6 to 30 inches on the primary structure.
- b. Eaves on accessory buildings, dormers, and other smaller structures should overhang at least 4 inches.
- c. Exposed timber eaves should be a minimum of three inches by three inches in dimension.
- d. Buildings may satisfy these overhang requirements with a CORNICE or similar form projecting horizontally from near the top of the building wall between 6 and 30 inches horizontally beyond the building wall.

4. Other Elements

- a. Roof vents or other roof-oriented equipment are encouraged only on the roof plane opposite the STREET-SPACE or when shielded from STREET-SPACE view by the building's parapet wall.

5. Street Walls

A. Purpose and Intent

The STREET-SPACE is physically defined by buildings, walls, or fences. Land should be clearly public or private—in public view or private and protected. STREET WALLS establish a clear edge to the STREET-SPACE where the buildings do not. These guidelines include masonry walls that define outdoor spaces and separate the STREET-SPACE from the private realm (e.g. parking lots, trash cans, gardens, and equipment). All STREET WALL faces should be as carefully designed as the building FAÇADE, with the finished side out (i.e. the “better” side facing the STREET-SPACE).

B. Applicability

The following standards are encouraged for all STREET WALLS that are CLEARLY VISIBLE FROM THE STREET-SPACE.

C. Illustrations

Photographs are provided as illustrations of intent. The illustrations and statement on this page are advisory only.



Left to right: STREET WALL defining private yard; STREET WALL with a door; STREET WALL with gate between FAÇADES



Left to right: STREET WALL with gates shielding service area; STREET WALL fronting an unbuilt frontage

D. Materials

The following materials are encouraged on STREET WALLS and gates:

1. Walls

- a. Native/regional stone and EQUIVALENT OR BETTER imitation stone;
- b. Brick;
- a. Stucco on concrete block or poured concrete (only when a brick or stone COPING is provided);
- c. A combination of materials (e.g. stone piers with brick infill panels);
- d. Native/regional stone and EQUIVALENT OR BETTER imitation stone;
- e. Wood (where configured to be effectively opaque); or

2. Gates

- b. Metal (wrought iron, welded steel and/or black aluminum) - may also be used for FENESTRATION in the wall itself; or
- c. Wood.

E. Configurations and Techniques

1. STREET WALLS along any unbuilt REQUIRED BUILDING LINE should be built to the height and length specified in the BUILDING FORM STANDARD.
2. COPING, or similar finish cap, should project between one inch and four inches from the face of the street wall.
3. Metal work may additionally be treated to imitate a copper patina.

6. Windows and Doors

A. Purpose and Intent

The placement, type, and size of windows and doors on the FAÇADE largely establishes the scale and character of the STREET-SPACE. For retail buildings, windows allow interplay between the SHOPFRONT interiors and the STREET-SPACE. Commercial uses (especially restaurants and retail establishments) benefit from exposure to the passers-by and the STREET-SPACE benefits from the visual activity. For residences, windows foster the “eyes on the street” surveillance which provides for the security and safety for the area.

Windows should be divided by multiple panes of glass to provide a pedestrian scale.

B. Applicability

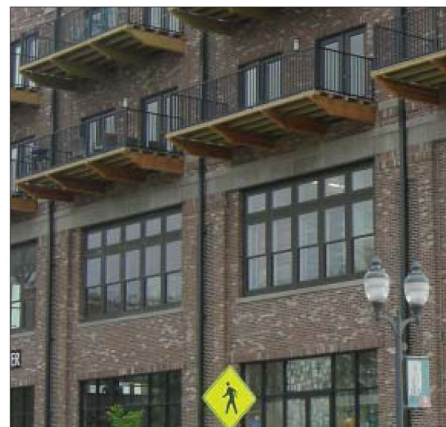
The standards in this section are encouraged for any window or door that is CLEARLY VISIBLE FROM THE STREET-SPACE.

C. Illustrations

Photographs are provided as illustrations of intent. The illustrations and statement on this page are advisory only.



Left to right: Door with TRANSOM; Multi-paned SHOPFRONT windows and glass doors; SHOPFRONT window



Left to right: Grouped windows; SHOPFRONT frontage; Grouped windows

D. Materials

Only the following materials are permitted on windows and doors:

1. Window glass should be clear, with light transmission at the GROUND STORY at least 90 percent and for the upper STORIES at least 75 percent (modification as necessary to meet applicable building and energy code requirements);
2. Specialty windows (only a single FENESTRATION opening) per FAÇADE COMPOSITION maximum) may utilize stained or opalescent glass, or glass block;
3. Doors should be made of wood, clad wood, glass, steel, or any combination; and
4. Shutter materials should be wood or clad wood.

E. Configurations and Techniques

1. All Windows

- a. The horizontal dimension of the opening should not exceed the vertical dimension except for SHOPFRONT TRANSOM windows;
- b. Windows may be grouped horizontally if each grouping (maximum five per group) is separated by a MULLION, column, pier or wall section that is at least seven inches wide;
- c. Exterior shutters, if applied, should be sized and mounted appropriately for the window (one-half the width), even if inoperable;
- d. For SHOPFRONTS in the Form District, the maximum dimensions for glass panes should be 120 inches vertical by 60 inches horizontal;
- e. The maximum dimensions for glass panes should be 60" vertical by 36" horizontal in all other frontages;
- f. Window panes should be recessed behind their FAÇADE surface a minimum of three inches, except for BAY WINDOWS, and SHOPFRONTS; and

- g. Snap-in MULLIONS and MUNTINS are encouraged for FENESTRATION.

2. Upper-Story Windows

Windows located above the GROUND STORY FAÇADE should meet the following:

- a. Windows should be triple-hung, double-hung, single-hung, hopper, AWNING, or casement windows.
- b. Fixed windows should only as part of a window grouping that includes an operable window.
- c. Egress windows should be installed as required by the applicable building code.

3. Doors

- a. Double-height entryways (those that span more than one STORY) should not be allowed.
- b. General Flex and Storefront FAÇADE doors should not be recessed more than four feet behind their FAÇADE/SHOPFRONT and, in any case, should have a clear view and path to a 45-degree angle past the perpendicular from each side of the door into the STREET-SPACE.

Part B. Signage Guidelines

1. Purpose and Intent

Signs along frontages within the Form Districts should be clear, informative to the public and durable. Signs should be scaled and detailed for these mixed-use, pedestrian-oriented areas; and not for high speed automobile traffic. Signage that is glaring or too large creates distraction, lessens the pedestrian experience, and creates visual clutter.

2. Applicability

The following standards are encourage for all signage that is CLEARLY VISIBLE FROM THE STREET-SPACE.

3. Illustrations

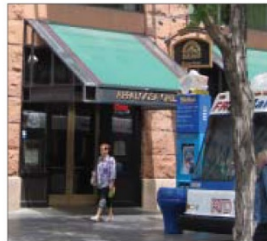
Photographs are provided as illustrations of intent. The illustrations and statement on this page are advisory only.



Left to right: Wall sign; Neon sign, within the SHOPFRONT



Left to right: Wall sign above entry; Sign band, window signs; Projecting signs



Left to right: Wall sign, AWNING; Projecting sign; AWNING sign; AWNING and wall signs

A. General Standards

1. Wall signs are encouraged within the area between the FIRST FLOOR ceiling and the second STORY floor line, within a horizontal band not to exceed three feet in height. This band should not be higher than 24 feet or lower than 12 feet above the adjacent sidewalk.
2. Letters should not exceed 24 inches in height or width and two inches in relief (depth). Signs should not come closer than two feet to an adjacent COMMON LOT LINE.
3. A single masonry or bronze plaque should be placed in the building's CORNICE/parapet wall or under the eaves, and above the upper STORY windows. Any such plaque should not be no larger than a rectangle of 18 square feet in size.
4. Projecting signs and marquee signs should be a maximum of three feet by four feet, vertical or horizontal with a minimum nine feet CLEAR HEIGHT above the sidewalk and may be hung within the permitted area between the FIRST FLOOR ceiling and the second STORY floor line, perpendicular to the front yard setback or from an AWNING.
5. Temporary A-Frame Boards – should not exceed 36 inches in height, 24 inches in width and 24 inches in depth (spread). Signs should occupy the DOORYARD area only and shall not occupy the CLEAR SIDEWALK.

B. Discouraged Signs:

Outdoor advertising signs, roof signs, free-standing pole signs, monument signs, any kind of animation, and signs painted on the exterior walls of buildings. No digital, flashing, scrolling, traveling, animated, or intermittent lighting shall be on the exterior of any building whether such lighting is of temporary or long-term duration. Portable or wheeled signs and advertising located outside any building are also discouraged.

C. Awnings and Overhangs

1. AWNING overhangs should have a minimum of ten feet CLEAR HEIGHT above the sidewalk and be minimum of five feet deep, measured from the FAÇADE. The maximum depth is to back-of-curb or the far (street) side of the CLEAR SIDEWALK edge, whichever is less.
2. Only the following materials are promoted: canvas or equivalent (no shiny or reflective materials), metal or glass.
3. Internal illumination through the AWNING or overhang is discouraged.
4. Lettering on AWNINGS should be limited to six inches in height on the outside edge/vertical face of the AWNING. Lettering and/or signs on AWNINGS should not be above the GROUND STORY.

Part C. Lighting, Mechanical and Service Area Guidelines

1. Purpose and Intent

Appropriate lighting is desirable for night-time visibility, safety, and decoration. However, lighting that is too bright or intense creates glare, hinders night vision, and creates light pollution. Pedestrian-scaled streetlights should occur along all streets within a Form District. “Cobra-head” highway-type fixtures shall be limited to major intersections and only when absolutely necessary. Lighting elements should that cast a clearly/perceptively unnatural spectrum of light (such as low pressure sodium) should not be used. LED, metal halide, or halogen elements with a spectrum of light more perceptively “natural” are preferred.

Mechanical includes any heating, ventilation, and air conditioning (HVAC) or electrical machinery but also includes air compressors, hoods, mechanical pumps, exterior water heaters, water softeners, utility company transformers, meters or boxes, trash compactors, dumpsters, storage tanks, and similar elements. These should not be located in any public areas or be visible from the street.

2. Applicability

The guidelines in this section apply to all properties in a Form District.

3. Illustrations

Photographs are provided as illustrations of intent. The illustrations and statement on this page are advisory only.



Left to right: Street light luminaire and banner; Pedestrian-oriented street lights; Not encouraged within the STREET-SPACE (The examples in the right column are mechanical equipment arrangements that are only acceptable away from and/or not visible from a STREET-SPACE, e.g. within a SHARED DRIVE or hidden from view).

A. Lighting

1. Streetlights should be coordinated by the Department of Public Works, MDOT and/or ICRD. Streetlights should be located on each side of the STREET-SPACE and between 9 feet and 16 feet above grade.
2. Streetlight and STREET TREE placement should be coordinated and should sit no less than 10 feet apart from one another.
3. Exterior lights at the building FAÇADE (maximum 100-watt incandescent or equivalent lumens) should be mounted between 8 feet and 12 feet above the adjacent sidewalk. These fixtures should illuminate the DOORYARD and CLEAR SIDEWALK area, and should be shielded or aimed in such a way that they do not direct light upward, or out of the Form District.
4. All lots with SHARED DRIVES should have lighting fixtures within five feet of the SHARED DRIVE. These fixtures should illuminate the SHARED DRIVE, be from 9 to 16 feet in height, and not cause glare into adjacent lots.
5. High-intensity discharge (HID) or fluorescent lights (excepting compact fluorescent bulbs that screw into standard sockets) should not be used on the exterior of buildings.
6. Directional lights, which cast light in a specific direction to highlight a particular surface, or floodlights (maximum 100-watt incandescent or equivalent) may be used to illuminate SHARED DRIVES, parking garages and working (maintenance and service) areas, but should be shielded or aimed in such a way that they do not shine into other lots, the STREET-SPACE, or direct light out of the Form District.

7. Flood or uplighting should not be used to illuminate private building walls. Accent lighting may be permitted on CIVIC BUILDINGS or monuments, to highlight architectural features (such as church steeples).
8. Site lighting should be of a design, height and location so as to illuminate only the lot. An exterior lighting plan should be approved as consistent with these guidelines by the Township.
9. Temporary holiday lighting is exempt from these regulations, in accordance with other Township standards.

B. Mechanical Equipment

1. The following should be placed behind and away from any front yard setback may not be stored or located within nor CLEARLY VISIBLE FROM THE STREET-SPACE: air compressors, mechanical pumps, exterior water heaters, water softeners, utility and telephone company transformers, meters or boxes, dumpsters, storage tanks, and similar equipment.
2. Utility lines, fiber optic, etcetera, should be placed underground, under the street pavements or under an alley or SHARED DRIVE pavement.

Part D. Streetscaping and Landscaping Guidelines

1. Purpose and Intent

- A. These guidelines include the public right-of-way include streetscape and landscaping elements and have the following goals:
1. To help implement the Township's Master Plan, the Street Master Plan, and other adopted planning document as well as any future Corridor Improvement Authority Plans.
 2. To ensure the coherence of the STREET-SPACE as an environment that encourages and facilitates walking and bicycling as safe, accessible, and healthy travel options.
 3. To ensure the design and use of public and quasi-public spaces supports the intended character of the district and complements private development.
 4. To contribute to ultimate sustainability. Native (and non-native adapted) trees and plants contribute to privacy, the reduction of noise and air pollution, shade, maintenance of the natural habitat, conservation of water, and storm-water management.

2. Applicability

- A. This section includes design standards for public road rights-of-way under the jurisdiction of the agencies above. The standards herein are intended to be applied to the right-of-way in front of private development but also to be considered by the road agency as part of any improvement or reconstruction of the transportation systems in the Form Districts.

3. Street Trees

- a. Each STREET-SPACE should have STREET TREES planted generally 3 to 3½ feet from the back of the curb, at an average spacing not greater than 30 feet on center. STREET TREE spacing should not exceed 45 feet on center except where necessary for transit stops or stations, curb cuts, fire hydrants and other infrastructure elements. Encouraged STREET TREE planting area configurations are specified below.
- b. Tree planting area suggestions are as follows:
 1. STREET TREE planting areas should be at grade or not greater than six inches above or below the sidewalk
 2. Soil surface area should not be less than 110 square feet per isolated tree or 90 square feet per tree for connected (TREE LAWN) situations.
 3. No dimension of the soil surface for any STREET TREE area should be less than 5.5 feet unless otherwise specified in this District.
 4. The above guidelines may be met through the use of bridged slab or other techniques that clearly exceed these standards in the fostering of vital and long-lived STREET TREES.
 5. At planting, STREET TREES should be at least 2.5 inches in diameter at designated breast height (DBH) and at least 12 feet in overall height. Species should be selected from the STREET TREE list (see Tree Lists). The Township may designate the appropriate tree species for a particular STREET-SPACE.
 6. Any unpaved ground area should be planted with groundcover or flowering vegetation, not to exceed 12 inches in height. STREET TREES should be "limbed up" as they gain appropriate maturity so as to not interfere with pedestrian or truck travel (minimum 7 feet clear over the sidewalk and 14 feet over any travel lanes) and to maintain visibility.

4. Streetscape Elements and Materials

1. At the time of development, or as coordinated by MDOT (Grand River Ave.) or the ICRD, the developer should install Sidewalks or shared pathways on the side of the STREET-SPACE being developed.
2. Sidewalks are encouraged to be a minimum of five feet wide and shared pathways a minimum seven feet wide (refer to the Street Standards and Township Pathways Plan).
3. Street furniture should be provided that is simple, functional, and durable and meets Township standards. Generally these elements should be installed on both sides of the street and aligned with STREET TREES (out of the CLEAR SIDEWALK area). Street furnishings should include but are not limited to elements such as Township-approved banners, art, and hanging baskets

on light poles, planter boxes, benches, litter receptacles, newspaper racks, sidewalk cafe seating where space permits, sidewalk advertisements or civic kiosks, and CATA-approved transit stop amenities.

4. Streetlights to support a walkable and safe district are encouraged for larger-scale developments. In other cases, lighting is funded through a Special Assessment District. The selection of streetlights and street furnishings should be harmonious between individual elements in order to create a consistent aesthetic language for the STREET-SPACE as a whole or by District. The Board of Water and Light or Consumers Power have a selection of street light fixtures available.
5. Streetscape elements should consist generally of high quality and well-detailed construction materials including clay or concrete permeable brick pavers, natural stone or granite curbs and pavers, and finely detailed cast concrete.

5. Tree Lists

A. General

STREET TREES and public space trees should be selected from an approved list provided by Meridian Township or Ingham County. If no accepted or applicable standards exist, STREET TREES should be selected from the following list and approved by Meridian Township during the plan review process.

B. Street Trees

STREET TREES are part of an overall STREET-SPACE plan designed to provide both canopy and shade and to give special character and coherence to each street. The desired aesthetic should be achieved through the use of native and/or proven hardy adapted species. Appropriate STREET TREE species may change over time and acceptable species may be periodically amended by the Township and/or Ingham County.

C. Private Space Trees

No trees, or other plant species that have been identified as invasive by the Michigan Invasive Plant Council may be planted in any outdoor location within the Form Districts.

Street Tree List (Large Canopy Trees – mature height 60 feet and above)	
<i>Acer rubrum</i> 'Sun Valley'	Sun Valley Red Maple
<i>Acer saccharum</i> Green Mountain®	Sugar Maple
<i>Ginkgo biloba</i> 'Autumn Gold'	Ginkgo (male only)
<i>Gleditsia triacanthos</i> var. <i>inermis</i> 'Skyline'	Skyline Honey Locust
<i>Platanus occidentalis</i> 'Bloodgood'	London Plane tree
<i>Quercus alba</i>	White Oak
<i>Quercus bicolor</i>	Swamp White Oak
<i>Quercus macrocarpa</i>	Bur Oak
<i>Quercus palustris</i>	Pin Oak
<i>Quercus imbricata</i>	Shingle Oak
<i>Quercus rubra</i>	Northern Red Oak
<i>Tilia Americana</i>	Basswood/American Linden
<i>Ulmus hollandica</i> 'Groenveldt'	Groenveldt Elm
<i>Ulmus americana</i> 'libertas', 'princeton', 'forge', 'delaware'	Elm
<i>Ulmus x spp.</i> 'Prospector'	Elm
<i>U. x spp.</i> 'Patriot'	Elm

6. Squares and Civic Greens

A. Intent

1. These guidelines apply to those spaces that are either publicly owned or publicly accessible, as designated on the REGULATING PLAN.
2. PUBLIC SPACES such as SQUARES and CIVIC GREENS should be situated at prominent locations. The plants and trees of SQUARES and CIVIC GREENS provide a landscape and civic architecture that complement the surrounding private building architecture.
3. SQUARES are active pedestrian centers. CIVIC GREENS are spaces intended for less intensive foot traffic.
4. A new CIVIC GREEN OR SQUARE shown on the REGULATING PLAN may be relocated in the general vicinity that meets the Purpose of such spaces to provide consolidated and functional open space.
5. Pervious paving materials (to allow oxygen for tree roots and absorb storm-water run-off) are encouraged in both SQUARES and CIVIC GREENS, and the percentage of impervious paving material is limited. (see *C. Materials and Configurations*)

B. Guidelines

1. SQUARES and CIVIC GREENS should be designed, planted and maintained according to the following requirements:

2. SQUARES and CIVIC GREENS should have at least 60 percent of their perimeter fronting public rights-of-way. Both should be surrounded by STREET TREES. Their dimensions should be no narrower than a 1:5 ratio and no SQUARE or CIVIC GREEN width or breadth dimension shall be less than 25 feet.
3. Appropriate to their high (pedestrian) traffic level SQUARES should be designed with a higher percentage of paved surface area.
4. A clear view through the SQUARE or CIVIC GREEN (from two to seven feet in height) is encouraged, both for safety and urban design purposes.

C. Materials and Configurations

1. General

- a. STREET TREES should be planted in accordance with *Section. 508, B. Street Trees*. They may be of a different species than the connecting streets. The ground surface level elevation should be between 0 and 18 inches above the top of the adjacent curb.
- b. The maximum slope across any SQUARE or CIVIC GREEN should not exceed ten percent.
- c. Except for tree trunks, streetlights, CIVIC USE BUILDINGS, public art or monuments, there should be a clear view between two and seven feet above grade. The foliage of newly planted trees may intrude into this area until the tree has sufficient growth to allow such a clear trunk height.
- d. Trees within a SQUARE or CIVIC GREEN may also be selected from the Public Space Tree Lists (see *Sec. 508 Tree Lists*).

2. Pedestrian Pathways

The area within a PEDESTRIAN PATHWAY should be a public access easement or public right of way. The easement width for these pathways should not be less than 20 feet with a paved walkway not less than 10 feet wide providing an unobstructed view straight through its entire length, except where otherwise specified on the REGULATING PLAN.

Part E. Street Design Guidelines

1. Applicability

- A. Guidelines in this section regarding design in the public road right-of-way are intended to ensure road design and reconstruction project complement the Form District. These guidelines are also subject to the standards and approval of the Ingham County Road Department (ICRD) or Michigan Department of Transportation (MDOT) as applicable.
- B. Establish the design principles for the complete STREET-SPACE.
- C. Roads within Meridian Township are under the jurisdiction of one of the following:
1. MDOT (Grand River Ave.)
 2. ICRD (all other public roads)
 3. Private roads and drives – owned and maintained by a private property owner or association (including front access lanes along Grand River Avenue)
- D. This section includes design standards for public road rights-of-way under the jurisdiction of the agencies above. The guidelines herein are intended to be applied to the right-of-way in front of private development but also to be considered by the road agency as part of any improvement or reconstruction of the transportation systems in the Form Districts.



The STREET-SPACE is a human and sustainable environment

2. Intent

- A. The guidelines have the following goals:
1. To help implement the Township's Master Plan, the Street Master Plan, the M-43 Access Management Plan and other adopted planning documents and any future Corridor Improvement Authority Plans.
 2. To promote the Township and MDOT Complete Streets policies.
 3. To ensure the coherence of the STREET-SPACE as an environment that encourages and facilitates walking and bicycling as safe, accessible, and healthy travel options.
 4. To improve pedestrian connectivity between destinations and CATA bus stops to support transit ridership.
 5. To ensure the design and use of public and quasi-public spaces supports the intended character of the district and complements private development.

3. Bus Stop Guidelines

One purpose of this district is to provide convenient access and amenities to support transit use. Developers are encouraged to work with the Capital Area Transportation Authority (CATA) to coordinate transit access, designation of new bus stops, relocation of existing bus stops, or bus stop enhancements. In general, the following standards shall apply:

- A. Pedestrian connections should be provided between building entrances and the public sidewalk or pathway system that links with a bus stop.
- B. Development within 660' (1/8 mile) of a bus stop in particular should be designed to support transit use.
- C. In some cases, CATA may work with a major land owner to provide transit access within a development or relocate a bus stop. In such cases, the internal site circulation must be designed to accommodate bus maneuvers.
- D. Developers with a bus stop along the frontage are encouraged to work with CATA to provide bus stop amenities.
- E. If a bus stop is within 660' (1/8 mile) of a development or redevelopment, developers should contact CATA for its potential relocation.

F. Locations:

1. Stops should be located in safe areas along streets for buses to stop and/or pull out to access the curb. Any in-street bus zone or pull-out areas should be sized to facilitate bus movements, and be at least 11-feet wide.
2. Stops should be located as closely as possible to the pedestrian access points of nearby trip-generating land uses or other activities, such as commercial centers, schools, employment sites or residential areas.

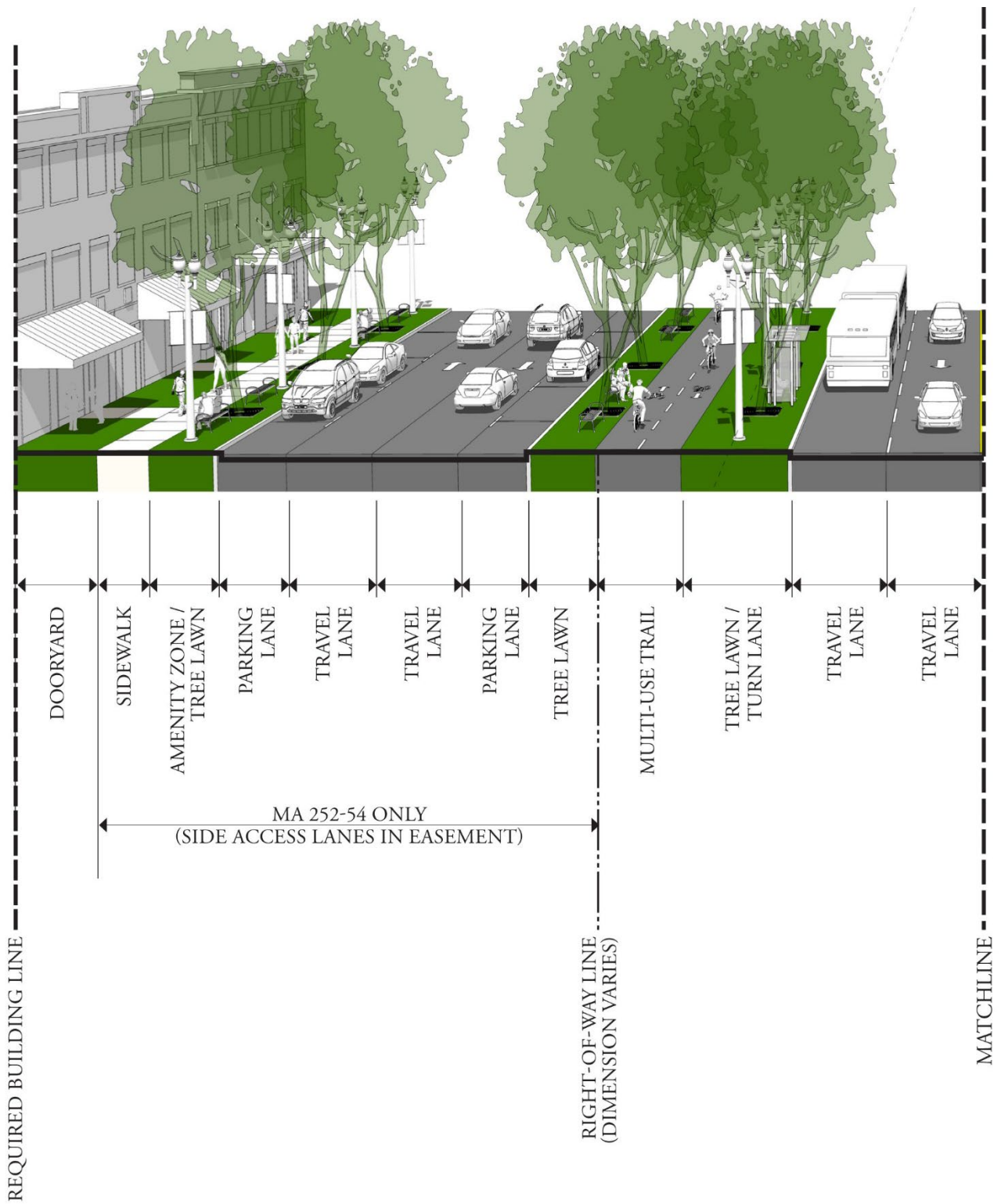
3. Stops should be positioned to be directly accessible by sidewalk, with any stop features positioned so as not to impede a minimum 5-foot clearance on the passing sidewalk.
4. Stops should be located near accessible and signed pedestrian crossing locations.
5. Stops should be located near lighting to allow for safe customer waiting experience during times of darkness.
6. To facilitate better operations, it is preferred that bus stops be located on the far side of signalized intersections. For these far-side stops, it is preferred that there is room for the front of the bus to stop a minimum of 80 feet past the intersection in order to give clearance to adjacent crosswalks.
7. Coordination with CATA is recommended to facilitate stop placement, relocation and design consistency.

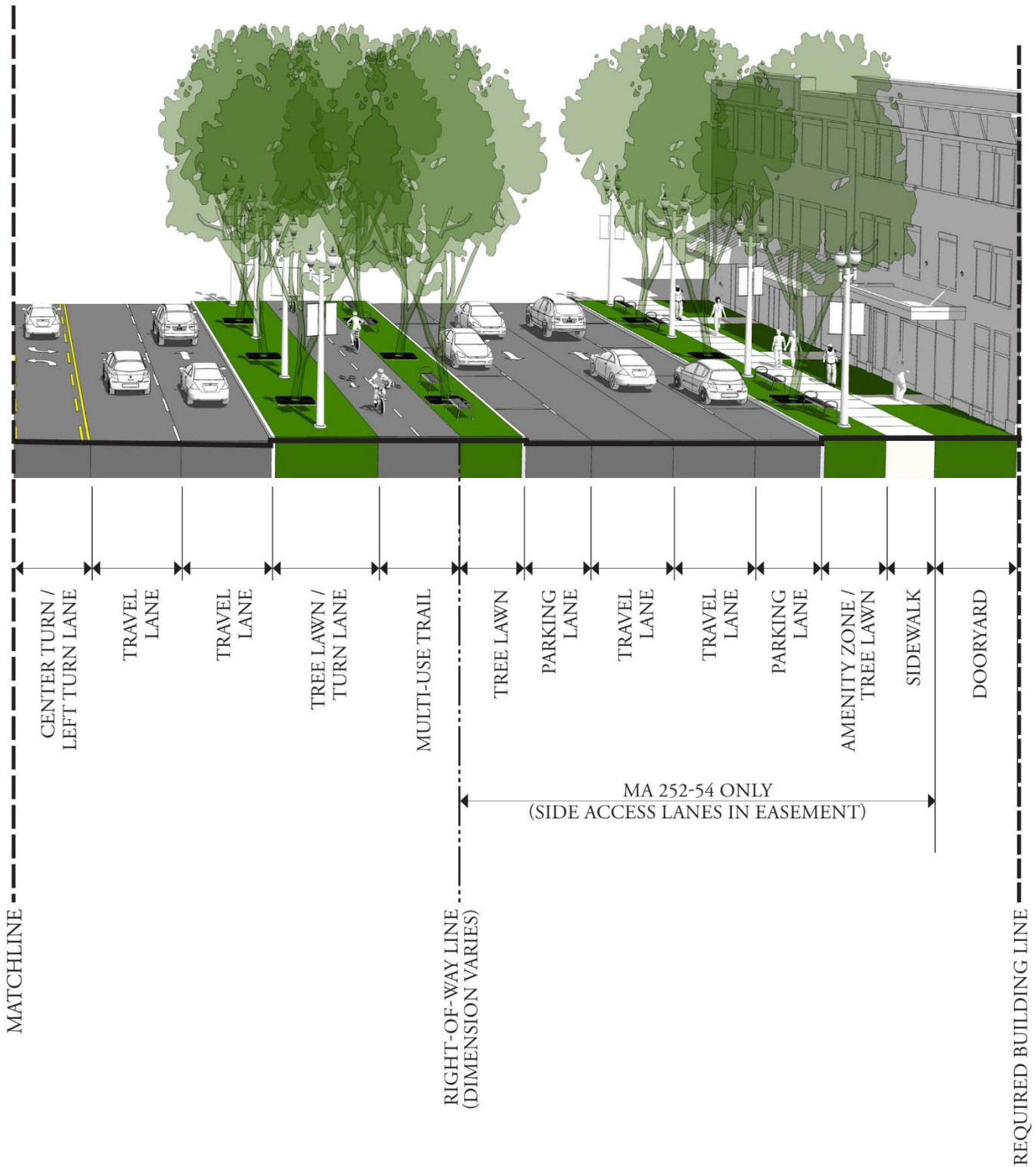
4. Pedestrian Crossing Guidelines

Distances along Grand River Avenue between current designated pedestrian crossings are not ideal to support a walkable district and provide safe access across the road to transit stops. To improve pedestrian travel, comfort and safety the following design features should be considered:

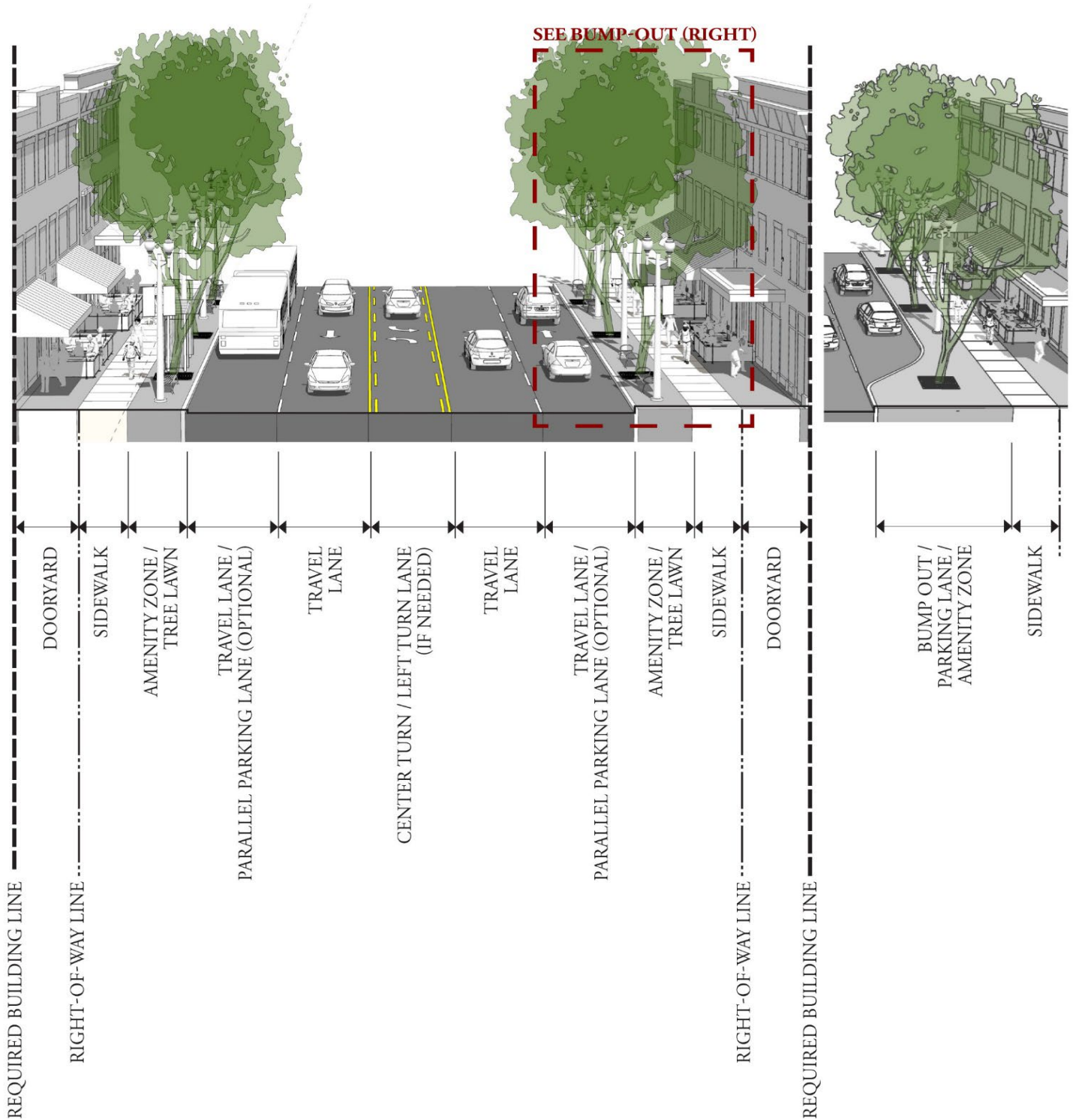
- A. Pedestrian crosswalks and transit stops should be located near each other to encourage crosswalk use by transit riders.
- B. Development design should encourage pedestrians to use designated pedestrian crosswalks and discourage crossings at other locations. Design elements such as door locations, other site access locations, sidewalk placement, streetscape amenity placement, and decorative walls or hedges can help direct pedestrians to desired crossing locations.
- C. Crosswalks may be needed within larger parking lots to connect with the public sidewalks or pathways. Such crosswalks may include treatments such as pavement markings, different pavement materials, signs, overhead beacons, curb extensions, crossing islands, and raised pedestrian islands.
- D. The MDOT “Guidance for Installation of Pedestrian Crosswalks on Michigan State Trunkline Highways” provides guidelines for crosswalks and mid-block pedestrian crossings.

5. Major Arterial Street Cross Section





6. Neighborhood Street Cross Section



Part G. Definitions

Defined terms are shown throughout the guidelines in SMALL CAPITAL LETTERS.

AWNING. A cantilevered, projected or suspended cover over the sidewalk portion of the STREET-SPACE, or a roof like covering, usually of canvas, metal, or similar material and often adjustable, placed over the sidewalk, windows, or doors to provide protection from sun and rain. It is distinguished from a canopy because it is not permanent, nor a structural portion or architectural feature of the building and does not support substantial weight.

BAY WINDOW. Generally, a U-shaped enclosure extending the interior space of the building outward of the FAÇADE (along its STREET-SPACE side).

CIVIC GREEN OR SQUARE. A public open space designated on the REGULATING PLAN. The term CIVIC GREEN is generally used to describe a formally configured small public lawn or park that is primarily unpaved. The term SQUARE is generally used to describe spaces that have more paved surface area.

CLEAR HEIGHT. Within a structure, the habitable distance between the floor and ceiling. For entrances and other external building features, the unobstructed distance from the ground/sidewalk to the lowest element above.

CLEAR SIDEWALK. An area within a STREET-SPACE, the portion of the sidewalk that must remain clear of obstructions and allow public passage.

CLEARLY VISIBLE FROM THE STREET-SPACE. A building element more than 40 feet from a REQUIRED BUILDING LINE or STREET-SPACE is by definition *not* CLEARLY VISIBLE FROM THE STREET-SPACE (such as elements facing a COMMON LOT LINE). Also, common or party walls are by definition *not* CLEARLY VISIBLE FROM THE STREET-SPACE.

COMMON LOT LINES. Lot lines shared by adjacent private lots.

COPING. The cap or covering on top of a wall.

CORNICE. A CORNICE (from the Italian CORNICE meaning “ledge”) is a horizontally overhanging element that crowns a building.

DOORYARD. The area within the STREET-SPACE, extending across the entire width of the lot, between the FAÇADE of the building and the CLEAR SIDEWALK portion of the sidewalk, which may be paved or planted.

EQUIVALENT OR BETTER. A building material or construction technique that has been determined, by the Director of Community Planning & Development Director, to be at least equal to, in appearance, durability, etc., or surpassing those expressly permitted herein.

FAÇADE (Building Face). The building elevation facing the STREET-SPACE. Building walls facing private interior courts, COMMON LOT LINES, and SHARED DRIVES are not FAÇADES (they are elevations).

FAÇADE COMPOSITION. The arrangement and proportion of materials and building elements (windows, doors, columns, pilasters, bays) on a given FAÇADE.

FENESTRATION. Openings in a wall, including windows and doors, allowing light and views between the BLOCK and/or building interior (private realm) and sidewalk and/or street exterior (PUBLIC REALM).

FIRST FLOOR. See GROUND STORY.

GROUND STORY. The first habitable level of a building at or above grade. The next **STORY** above the **GROUND STORY** is the second floor or **STORY**.

MULLION. A vertical structural member in a window.

MUNTIN. A strip of wood or metal separating and holding panes of glass in a window, less than 1" in thickness. **MUNTINS** divide a single window sash or casement into a grid system of small panes of glass.

PEDESTRIAN PATHWAY. A publicly accessible interconnecting paved way providing pedestrian and bicycle passage running from a **STREET-SPACE** to another **STREET-SPACE**, **SHARED DRIVE**, or an interior parking area.

PUBLIC REALM (STREET-SPACE). All space between fronting building **FACADES**, including streets, squares, plazas, parks, pedestrian pathways, sidewalks, parks)—including transit service operator passenger platform—but not within **SHARED DRIVES**.

REGULATING PLAN. The implementing plan for development within the form districts.

REGULATING PLANS designate the **BUILDING FORM STANDARDS**. The **REGULATING PLAN** also shows how each site relates to adjacent **STREET-SPACES**, the overall district, and the surrounding neighborhoods.

SHARED DRIVE. The public right-of-way or easement for vehicles and pedestrians that provides service access to the rear or side of properties, vehicle parking (e.g., garages), loading docks, utility meters, recycling containers, and garbage bins.

SQUARE. See **CIVIC GREEN**.

SHOPFRONT. That portion of the **GROUND STORY FAÇADE** intended for marketing or merchandising and allowing visibility between the sidewalk and the interior space.

STORY. That space within a building and above grade that is situated between one floor level and the floor level next above, or if there is no floor above, the ceiling or roof above.

STREET-SPACE (PUBLIC REALM). All space between fronting building **FACADES**, including streets, squares, plazas, parks, pedestrian pathways, sidewalks, parks)—including transit service operator passenger platform—but not within **SHARED DRIVES**.

STREET TREE. Used to define the **STREET-SPACE/** pedestrian realm and listed in the **STREET TREE List**. **STREET TREES** are of proven hardy and drought tolerant species and large enough to form a shade canopy with sufficient clear trunk to allow traffic to pass under unimpeded.

STREET WALL. A masonry wall which assists in the definition of the **STREET-SPACE** in the absence of a building. See the **BUILDING FORM STANDARDS** for height and gate specifications.

TRANSOM. **TRANSOM** or **TRANSOM window** refers to a **TRANSOM light**, the window over a structural crosspiece in a window or door opening.

TREE LAWN. A continuous strip of soil area—typically covered with grass, other vegetation, bridging pavement, or sometimes porous pavers—located between the back of curb and the **CLEAR SIDEWALK** and used for planting **STREET TREES** and configured to foster healthy **STREET TREE** root systems.